



Capital Construction



EAST SIDE ACCESS QUARTERLY REPORT TO THE FTA

January - February - March 2010

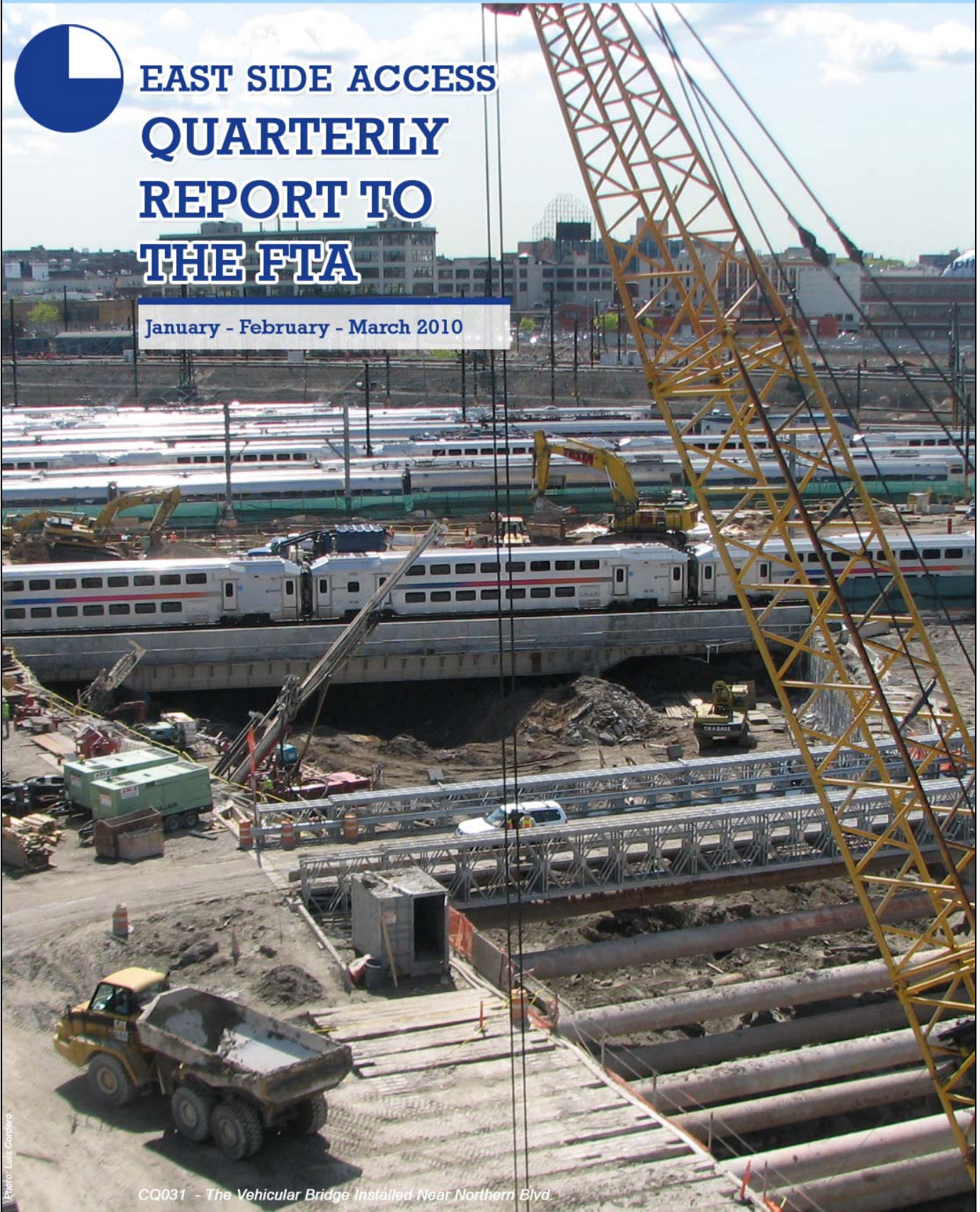


Photo: Luis Contreras

CQ031 - The Vehicular Bridge Installed Near Northern Blvd.

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Executive Summary

Status of ESA Budget and Schedule

BUDGET

TOTAL (millions)	<i>Budget</i>	<i>Awards</i>	<i>Invoiced</i>
	\$ 7,328.0	\$ 3,839.5	\$ 2,157.1
% Budget		52%	29%
% Awards			56%

Note: The budget and schedule reflect the September 2009 rebaseline as approved by the MTA Board

CONSTRUCTION ONLY (millions)	<i>Budget</i>	<i>Awards*</i>	<i>Invoiced</i>
	\$ 5,595.5	\$ 2,800.9	\$ 1,211.2
% Budget		50%	22%
% Awards			43%

*Awards reflect current contract values.

CONTINGENCY (millions)	<i>Contingency</i>
FFGA Baseline	\$ 855
Sept '09 Baseline	\$ 424
Current Contingency	\$ 463

COMPLETED CONTRACTS	Twenty three contracts valued at \$375.2 million
<i>Contracts Completed This Quarter</i>	<i>Emergency Work Environmental (CQE28-02)</i>

ACTIVE CONTRACTS	Twenty two contracts valued \$ 2.506 billion
<i>Contracts Awarded This Quarter</i>	<i>Northern Boulevard Crossing (CQ039)</i> <i>3rd Party Stimulus Materials (CS099)</i>

2010 PLANNED AWARDS (Budget)	2010 planned awards (budget) total approximately \$100 million. Major awards include:
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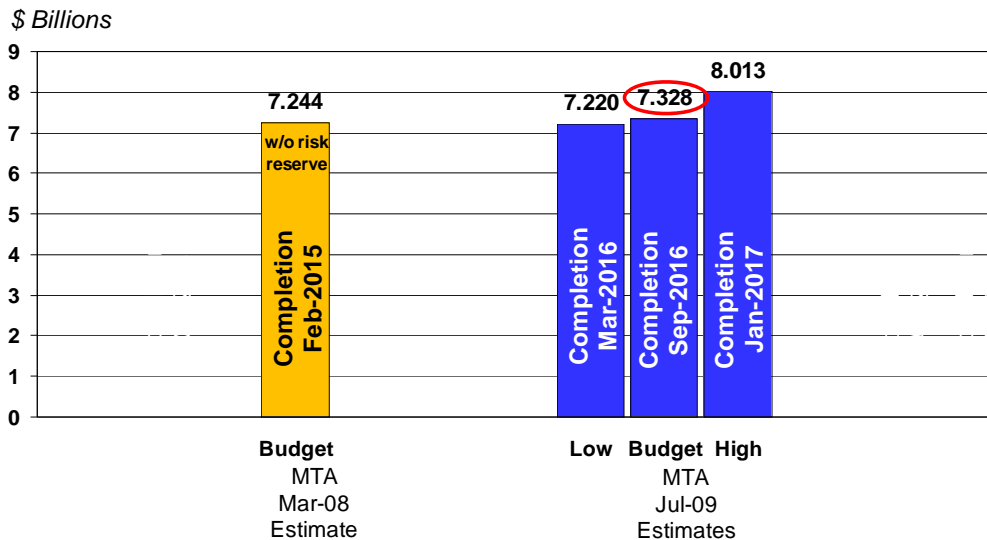
Elevators and Escalators (VM014) – Partial award anticipated

DESIGN	89% complete as of 3/31/10
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TOTAL PROJECT	25% complete as of 3/31/10
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1ST QTR '10 WORK PLAN INCLUDES:	1 contracts achieving 30% submittal 4 contracts achieving 60% submittal 2 contracts achieving 90% submittal 3 contracts achieving 100% submittal
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East Side Access – Budget & Schedule



Note: July 2009 values exclude \$463M in rolling stock costs, now budgeted elsewhere.

Highlights

- The contract *Northern Boulevard Crossing (CQ039)* was awarded to Schiavone-Kiewit for \$84.9 million. This contract includes construction of mined tunnel under Northern Boulevard, the IND underground subway and the BMT elevated subway
- The contract *Queens Bored Tunnels and Structures (CQ031)* option work of \$58.4 million for the Construction of Yard Lead Approach and Other Structures was exercised
- Contract *Queens Emergency Work – Environmental (CQE28-02)* reached Substantial Completion
- The Robbins TBM finished the first lower drive and the fifth of the eight tunnel drives in Manhattan under contract *Manhattan Tunnels Excavation (CM009)*
- Load transfer at Wellway #3 was completed under contract *Manhattan Structures Part 1 (CM019)*
- Completed demolition of Amtrak buildings 1, 2, 5 and 6 in Sunnyside Yard under contract *Queens Bored Tunnels and Structures (CQ031)*
- Microtunnel Run #8 was completed as the Microtunnel Boring Machine (60" diameter) advanced into the reception pit under contract *Harold Structure Part 1 and GO2 Substation (CH053)*
- Alan J. Paskoff, P.E. was promoted to Sr. Vice President and ESA Project Executive

Budget Review

- MTACC conducted a review of actual escalation rates for 2008 and 2009 and determined that the actual rates were much lower than assumed in the budget. The current budget was prepared using a base estimate in 2008 dollars which was then escalated to the mid-point of construction using the schedule that was in place at the time. Escalation rates of 4.62% and 4.25% for 2008 and 2009 respectively were used in the estimate. The review resulted in a weighted average actual rate for labor and material combined of 1.5%. MTACC will conduct similar reviews in the first quarter of each subsequent year to confirm actual escalation rates.

To reflect the difference between actual and projected escalation rates, first, the base contract estimates were updated to 2010 dollars. These estimates were then escalated to the mid-point of construction using the February 2010 Integrated Project Schedule. The savings realized by lower escalation for contracts *Manhattan Structures 2 (CM012)* and *GCT Concourse (CM014)* were added to the pre-bid contingency for these contracts. These contracts are undergoing their 60% design and constructability reviews; however, they have been the victim of some cuts during earlier exercises. These actions resulted in a net positive variance of \$51.9 million.

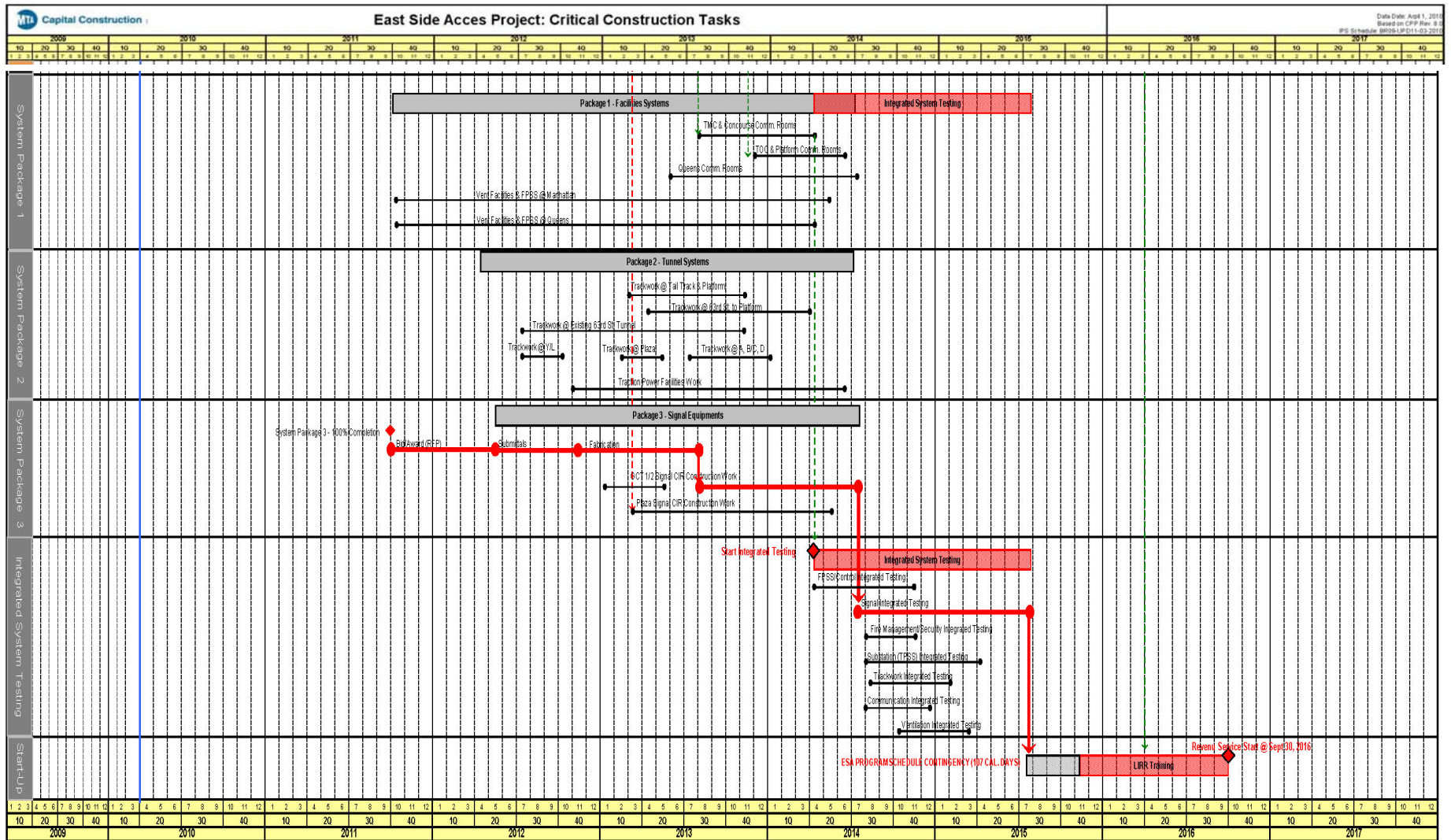
Three budget adjustments were subsequently implemented. The first adjusted the contract budgets to transfer the \$51.9 million in savings to construction reserve. The second adjustment reallocated \$27 million to *Harold Structures I (CH053)* to cover the projected shortfall in that contract. The third adjustment transferred \$1.3 million from reserve to *MNR Traction Power MODs & 13.2 kV Loop (FM216)*.

As of March 31st, contingency funds totaled \$462.5 million, an increase of 71.6 million from December 31, 2009. The escalation and schedule analysis noted above increased contingency by \$76.9 million, which was offset by another \$5.3 million in changes. Refer to the Status of Project Contingency Table on page 58.

American Recovery and Reinvestment Act (ARRA)

- The FTA awarded a grant to the MTA for \$195.4 million in funds made available through the ARRA. See page 56 for a list of the projects identified for ARRA funds.

Project Construction Critical Tasks (Page 2 of 2)



Active Construction Contracts Summary

NTP	Substantial Completion			Cost Expenditure % Complete		Current Budget (\$M)	Budget at Award (\$M)	Original Contract Award (\$M)	Current Approved Contract (\$M)	Estimate at Completion (\$M)	Remarks
	Plan		Forecast	Plan*	Actual						
	Orig	Revised									
MANHATTAN / BRONX											
CM009 – Manhattan Tunnels Excavations											Dragados-Judlau
7/10/06	07/10	04/12	09/12	68.3/69.7%	68.4%	450.3	449.4	427.9	443.4	450.3	Project team is working with contractor to mitigate delays and recover schedule.
CM019 – Manhattan Structures 1											Dragados-Judlau
04/01/08	03/12	04/12	09/12	34.1/34.2%	26.5%	779.8	756.0	499.9	528.4	779.8**	Project team is working with contractor to mitigate delays and recover schedule.
FMM19 – Manhattan Force Account Support – MNR											MNR Direct Force Account
04/01/08	03/12	03/12	09/12	N/A	54.0%	33.1	33.1	13.6	13.6	31.8	Force Account support for Manhattan construction.
CM008A – Madison Yard Site Clearance											Gramercy Group, Inc.
03/09/09	06/10	06/10	08/10	79.7/93.7	71.9%	40.9	40.9	39.0	39.7	40.9	Contractor is in full production and has submitted a recovery schedule to meet the contractual milestones which is currently under review.
CM004 – 44th St. Demolition & Construct Fan Plan Structure & 245 Park Ave. Entrance											Yonkers Contracting Company, Inc.
09/16/09	09/11	09/11	09/11	6.4/10.8%	6.3%	42.8	42.8	40.8	40.8	42.8	Progressing on schedule.
CM013 – 50th St. Vent Facility											CCA Civil-Hilmar International
01/4/10	06/12	06/12	06/12	1.0/3.2	1.5	99.1	99.1	94.4	94.4	99.1	Progressing on schedule.
FM216 – MNR Traction Power MODs & 13.2 kV Loop											TC Electric
09/25/08	03/10	06/10	06/10	85.3/91.7	71.3%	16.1	14.5	13.8	14.1	16.1	Project team is working with MNR to improve access to the requested tracks to recover the schedule. No impact to revenue service date.
CS790 – GCT Protection Works											Ad Tech Enterprises
0/23/08	10/09	06/10	07/10	90.0%	77.0%	13.0	12.8	10.3	12.0	13.0	Schedule has been adjusted due to MNR operations and ongoing blasting operation under contract CM019 limiting work areas.
QUEENS											
CS810 – Queens Instrumentation											Wang Technology
05/06/09	05/09	05/10	06/10	60.9/82.4%	44.8%	2.7	3.0	2.7	2.7	2.7	Behind schedule due to deployment of the railroad resources to other contracts based on program priorities.
CQ031 – Queens Bored Tunnels and Structures											Granite-Traylor-Frontier Joint Venture
09/28/09	09/12	09/12	09/12	16.5/23.5%	11.8%	756.8	756.8	659.2	717.6	756.8**	Original scope of work delayed so contractor can undertake slurry wall repairs (change order) prior to open-cut excavation. The recovery plan for open-cut excavation is being submitted.
CQ039 – Northern Boulevard Crossing											Schiavone/ Kiewit, a Joint Venture
02/03/10	10/11	10/11	10/11	TBD	TBD	89.2	89.2	84.9	84.9	89.2	Schedule under review.

*Late/ Early Plan

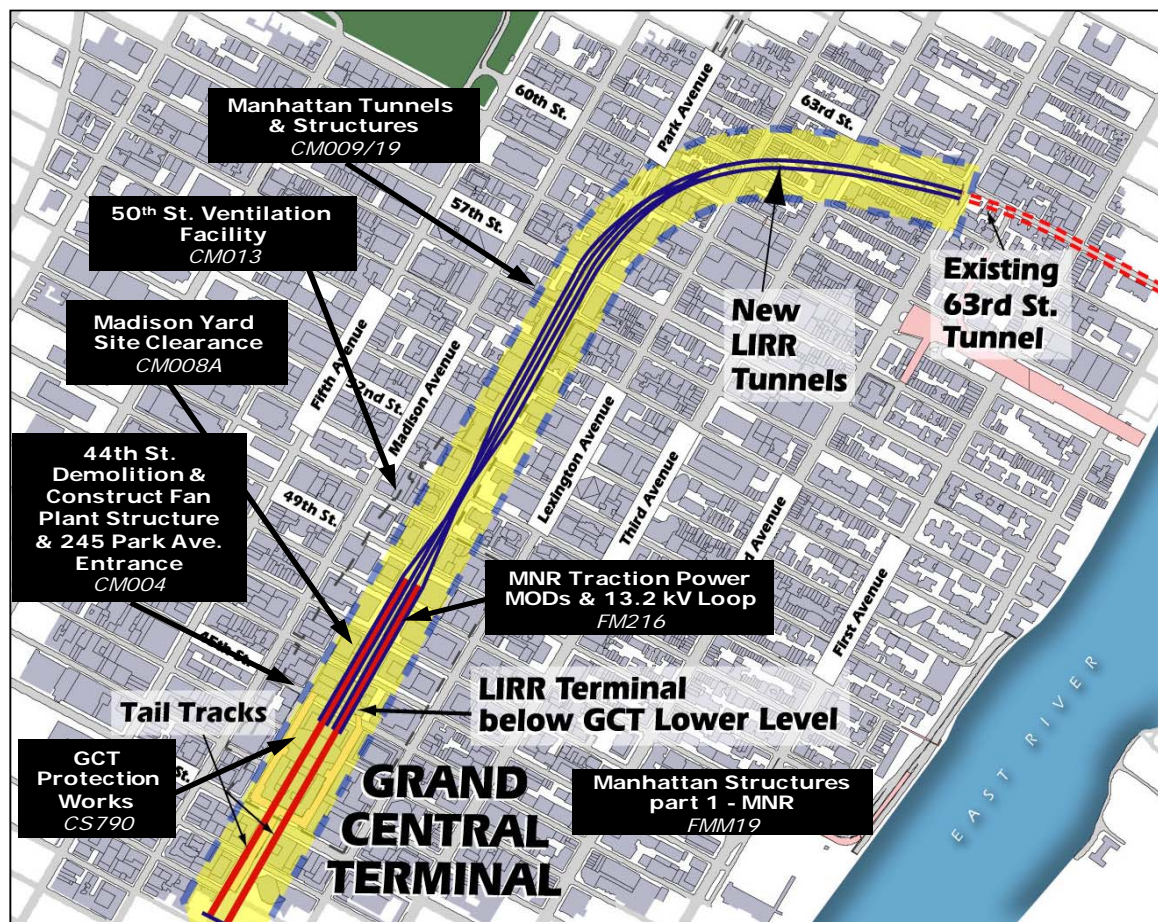
**EAC assumes contract options will be exercised

Quarterly Review Report – 1st Quarter ‘10

NTP	Substantial Completion			Cost Expenditure % Complete		Current Budget (\$M)	Budget at Award (\$M)	Original Contract Award (\$M)	Current Approved Contract (\$M)	Estimate at Completion (\$M)	Remarks
	Plan		Forecast	Plan*	Actual						
	Orig	Revised									
HAROLD INTERLOCKING											
CH053 – Harold Structures Part 1 and GO2 Substation											Perini Corp.
01/02/08	01/12	01/12	01/12	27.0/34.3%	35.4%	152.7	144.1	137.3	148.5	179.7	Contractor submitted a draft of the re-baseline schedule and it has been conditionally accepted.
CH054A – Harold Structures Part 2A											Perini Corp.
08/24/09	12/10	12/10	12/10	15.6/29.5%	6.8%	27.1	27.1	21.8	21.8	27.1	Project team is working with the contractor to assess and mitigate schedule impacts due to weather and unforeseen field conditions.
FHA01 – Harold Stage 1 – Amtrak											Amtrak Direct Force Account
06/1/07	09//09	01/12	01/12	51.8/55.6%	51.8%	16.8	12.1	12.4	13.9	16.8	Schedule is revised based upon CH053 rebaseline schedule.
FHA02 – Harold Early Stage 2 – Amtrak											Amtrak Direct Force Account
12/15/08	08/10	08/10	03/10	51.7/73.5%	96.1%	4.1	4.1	4.1	4.1	3.8	Progressing ahead of schedule (early stage 2) and under budget (partial budget).
FHL01 – Harold Stage 1 – LIRR											LIRR Direct Force Account
6/29/07	10/11	10/11	10/11	52.0/60.0%	51.9%	22.0	28.8	28.8	20.8	22.0	Schedule is revised based upon CH053 rebaseline schedule.
FHL02 – Harold Early Stage 2 – LIRR											LIRR Direct Force Account
08/17/09	04/10	04/10	05/10	67.0/86.6%	72.6%	5.1	5.1	5.1	5.1	4.8	Progressing on schedule (early stage 2 - partial budget).
FHA62 – F Interlocking CIH											Amtrak Direct Force Account
09/11/08	08/10	08/10	02/11	70.4/94.3%	63.0%	8.2	8.2	6.9	6.9	8.2	Schedules have been revised to reflect delivery schedule changes. No impact to follow-on contracts.
VHA02 – Procure Harold Material Stage 2 – Amtrak											Amtrak Procurement
01/09	08/13	08/13	08/13	9.0%	9.0%	13.2	13.2	6.0	6.0	13.2	Progressing on schedule.
VHL02 – Procure Harold Material Stage 2 – LIRR											LIRR Procurement
01/09	08/13	08/13	08/13	47.0%	47.0%	26.2	26.2	6.0	6.0	20.6	Progressing ahead of schedule.
CS099 – Advanced Procurement of Third Party Materials											Third Party Procurement
01/09	12/10	12/10	12/10	55.2%	55.2%	37.8	37.8	17.2	17.2	31.1	Progressing on schedule.
VH051 (Part 1) – Harold and Point CILs											Ansaldo STS USA, Inc
05/11/09	06/12	06/12	09/13	3.8/10.0%	7.0%	30.8	30.9	25.8	25.8	30.8	Deliveries of CILs have slipped but will not impact ESA project end date.
VH051 (Part 2) – Harold Tower Supervisory Control System											ARINC, Inc.
02/03/09	05/12	05/12	05/12	50.0/61.0%	60.0%	7.1	7.1	5.4	5.4	7.1	Progressing on schedule.

*Late/ Early Plan

Manhattan Active Contracts



Manhattan Tunnels Excavation (CM009) – The Manhattan tunnels excavation contract includes procurement of two 22 foot diameter rock tunnel boring machines (TBM), installation of temporary utilities, excavation of chambers and wyes, TBM (4 drives) excavation of approximately 25,000 linear feet (lf) of tunnels and final concrete lining of 11,600 lf of the running tunnels.

Manhattan Structures (CM019) – Excavation and lining of four tunnel drives, shafts, escalator well-ways, air plenums, cross-passages, and caverns mainly under Grand Central Terminal (GCT).

Manhattan Structures Part 1 – MNR (FMM19) – MNR Railroad resources in support of the contracts CM019, CM008A, CM004 and General Conditions contracts in the vicinity of GCT. These resources include both direct and indirect labor, worktrain support, and rack outages.

Madison Yard Site Clearance (CM008A) – Dismantle rail, ballast, concrete slab, crash walls, two platforms, and miscellaneous buildings; Demolish elevators and remove accompanying mechanical/electrical equipment. Duct removal and installation and fan work, and all other mechanical/electrical systems. Construct new MNR Commissary.

44th St. Demolition & Construct Fan Plant Structure & 245 Park Ave. Entrance (CM004) – 47 E. 44th Street: Demolish existing building, perform soil and rock excavation. Construct shell of new building. 245 Park Avenue Entrance: Conduct environmental abatement and demolition; construct new foundations, walls and supports, furnish and install escalator. Install architectural finishes, plumbing, fire protection, HVAC, lighting, power, fire alarm, and communication systems.

50th St. Ventilation Facility (CM013) – Construction of new ventilation building structure and vertical utility shaft form building foundation at 50th St.

MNR Traction Power MODs & 13.2 kV Loop (FM216) – Furnish, deliver and install automated system for motor-operated disconnect switches (MODs). Scope of work also includes reconfiguration of 13.2kV Traction Power feeder cables.

GCT Protection Works (CS790) – Make all necessary concrete repairs to roof slabs, columns and beams and install protective netting in GCT from 42nd thru 51 Streets (Upper Level Tracks 32 thru 42, Lower Level Tracks 113 thru 115).

Active Projects – Manhattan

CM009 – Manhattan Tunnels Excavation

Jan, Feb, Mar '10

Contractor: Dragados/Judlau
Construction Manager: Mike Pujdak

LONP#: 8
FTA Grant: NY-03-0344

Construction

Description: The Manhattan tunnels excavation contract includes procurement of two 22 foot diameter rock tunnel boring machines (TBM), installation of temporary utilities, excavation of chambers and wyes, TBM (4 drives) excavation of approximately 25,000 linear feet (lf) of tunnels and final concrete lining of 11,600 lf of the running tunnels.



Hopper at Cross Passage #4.



Mucking from the West Cavern.

Schedule and Cost

Progress:

TBM Mining Data (Active Drives)					
	Length (lf)	Start Date	Feb'10 lf	Cum lf	% Complete
EAST	3,902*	2/24/2010*	463	463	11.8
WEST	5,668	07/06/09 A	2,343	5,668	100.0

* Mining started on 2/24/2010 at GCT 5 concrete plug

** Length does not include concrete plug excavation

1. Completed backing up of SELI
2. Completed concrete pour at GCT 5 East Wye
3. Mining is ongoing at SELI lower level drive 3
4. Completed mining at West Bound (WB) lower drive 3
5. Completed backing up of Robbins TBM in West Bound (WB) lower level and prepared to start drive 4
6. Blasting ongoing (97 of 296 CY; 33% Completed) at Cross passage #2

Schedule: The current schedule shows a gain of 14 work days from last month with an overall delay of 6.3 months on substantial completion. Contractor submitted schedule update #16 which is under review.

Cost: The project is trending within budget. Monthly forecast projections indicate contingency values are sufficient. There are several cost saving opportunities under review including the use of shotcrete as a final liner for non-uniform structures and a revised geometry of the GCT 3 and 5 wyes.

Contract Modifications: As of 03/31/10, twenty two modifications, for a total of \$15,456,402 have been executed including the scope transfer for 38th St. Vent Plant (CM013B).

Milestones CM009*

Milestones Description	Current Approved Plan	Actual /Forecast
Advertise (RFP)	01/06	01/06 (A)
NTP	07/10/06	07/10/06 (A)
Start Tunnel Mining	09/25/07	09/25/07 (A)
Complete Drive #4 (SELI)	05/27/11	06/27/11
Substantial Completion	04/02/12	9/20/12
Close-Out	08/30/12	12/20/12

Schedule Progress	1st Qtr		Cumulative	
	Plan**	Actual	Plan**	Actual
Cost Expenditure				
Percent Complete	2.4/2.8	6.0	68.3/69.7	68.4

* Milestones reflects combine CM009/CM019 Rebaseline schedule

** Late/ Early Plan

Contract Costs (\$000) as of 03/31/10

	CM009	F/A Support	Total
Budget at Award	449,351	4,041	453,393
Current Budget	450,309	4,120	454,429
Original Award	427,954	3,924	431,878
Approved Changes	15,456	0	15,456
Current Contract Value	443,410	3,924	447,334
Invoiced Amount	304,152	1,217	305,369
Estimate at Completion	450,309	4,120	454,429

Critical Issues: Contractor/MTA to evaluate burying of one or both TBM's after completion of TBM mining to recovery schedule time.

Quarterly Review Report – 1st Quarter '10

Coordination Required

Real Estate: All properties and easements are secured for tunnel construction. MTA sent mailings to owners regarding contract NTP, zones of influence, pre-construction survey, instrumentation, and monitoring.

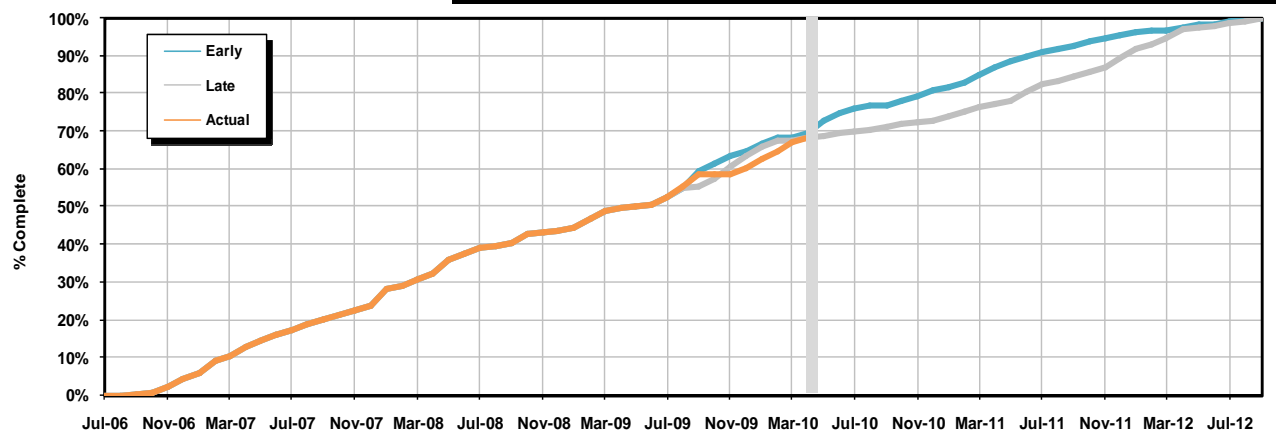
Railroad Agencies: Flagging and access protection are being provided by MNR and NYCT.

MOU: N/A

Permits: The contractor is responsible for securing the following:

- Blasting Permit – FDNY: Permit received
- Water Discharge Permit – NYCDEP: Permit received but contractor is seeking a variance
- Lane Closure Permits – NYCDOT: Permit received for 2nd Ave. and Northern Blvd

Progress (CM009)



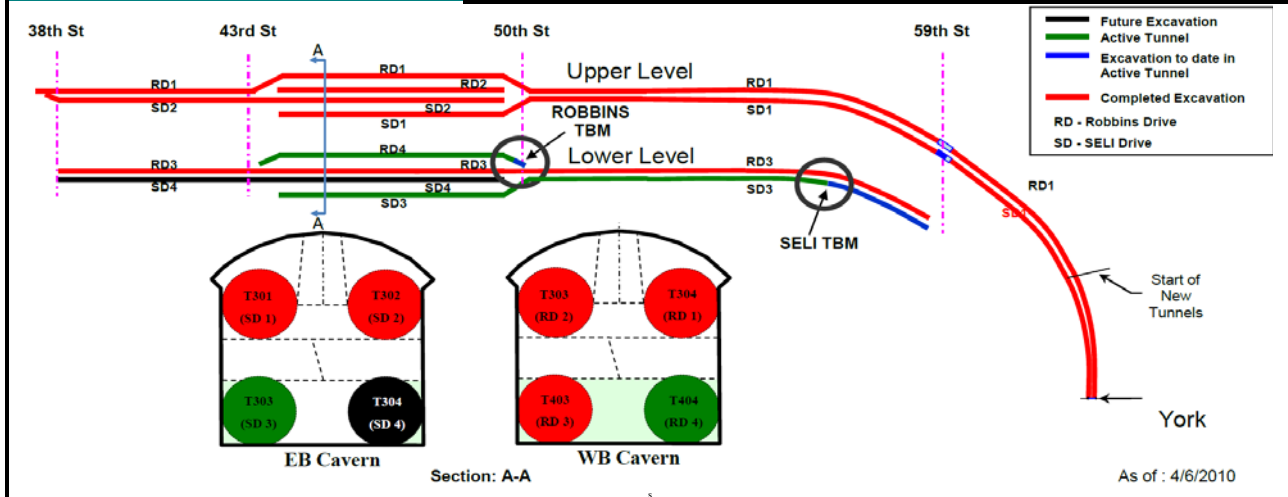
Safety

- Zero Lost Time Accidents this month
- Updated safety training, and reinforced best practices
- Continued ongoing safety walk-thru of sites

Safety Statistics* (Cumulative Incidents per 200,000 work-hours)			
	Project	Contract	BLS**('07)
Lost Time Accidents	1.92	2.67	2.60

*The project performance statistics are based on construction hours only
 **Bureau of Labor Statistics/National Industry Average

Site Map



CM019 – Manhattan Structures Part 1

Jan, Feb, Mar '10

Contractor: Dragados-Judlau

Construction Manager: Mike Pujdak

FTA Grant: NY-03-0344

Construction

Description: Excavation and lining of four tunnel drives, shafts, escalator well-ways, air plenums, cross-passages, and caverns mainly under Grand Central Terminal (GCT).



Hoeram Excavation at Shaft #2.



Ongoing Excavation at Wellway #4.

Schedule and Cost

Progress:

WB GCT Main Cavern Excavation					
Segment	Cut #	Total (CY)	1 st Qtr (CY)	Cum (CY)	% Complete
Total		95,832	7,435	16,331	17.0
Top Heading	1 & 1A	9,098	3,360	6,188	68.0
	2 & 2A	9,098	3,221	5,003	55.0
	3	12,830	730	2,526	20.0
Bench I	4 & 4A	12,098	124	2,614	22.0
Bench II	5	16,058	0	0	0.0
	6	16,058	0	0	0.0
Bench III	7	20,592	0	0	0.0

EB GCT Main Cavern Excavation					
Segment	Cut #	Total (CY)	1 st Qtr (CY)	Cum (CY)	% Complete
Total		95,832	7,246	10,466	11.0
Top Heading	1 & 1A	9,098	3,381	3,382	37.0
	2 & 2A	9,098	3,597	3,597	40.0
	3	12,830	268	520	4.0
Bench I	4 & 4A	12,098	0	2,967	25.0
Bench II	5	16,058	0	0	0.0
	6	16,058	0	0	0.0
Bench III	7	20,592	0	0	0.0

CY - Cubic Yard

Shafts & Escalators Excavation				
Location	Total (CY)	1 st Qtr (CY)	Cum (CY)	% Complete
Shaft 5	1,695	0	1,334	78.8
Shaft 4	4,389	3,066	4,062	93.0
Shaft 3	3,533	1,686	3,397	96.0
Access tunnel # 3	920	590	590	64.0
Shaft 2	1,613	123	123	8.0
WW 4 & Slope	4,138	256	1,124	27.0

- Completed load transfer at Wellway #3 and continued load transfer at Wellway #2
- Started excavation at Shaft # 2
- Continued installation of mucking system at Wellway # 4
- On-going preparation to blast at access tunnel # 4
- Excavated 150 CY at GCT 4 West Bound in concrete plug area
- Completed installation of crusher and conveyor to support drill and blast excavation

Schedule: The current schedule shows a gain of 14 work days from last month with an overall delay of 6.3 months on substantial completion. Contractor submitted Update # 16 which is under review.

Cost: The contract was awarded for \$499 million. An option exists that when exercised will increase the contract value to \$734 million. The project is trending within budget. Monthly forecast projections indicate contingency values are sufficient after the incorporation of potential future changes. There are several cost saving opportunities being reviewed including the revised geometry of GCT 4.

Milestones CM019*

Milestones Description	Current Approved Plan	Actual/Forecast
Advertise (RFP)	02/07	02/07 (A)
NTP	04/02/08	04/02/08 (A)
Complete WW2 Load Transfer	04/11/10	04/11/10 (A)
Complete West Cavern Top Heading	12/29/10	11/5/10
Substantial Completion	04/02/12	9/20/12
Close-Out	08/30/12	12/20/12

Schedule Progress	1st Qtr		Cumulative	
Cost Expenditure	Plan**	Actual	Plan**	Actual
Percent Complete	8.3/11.5	7.2	34.1/34.2	26.5

* Milestones reflects combine CM009/CM019 Rebaseline schedule

**Percent complete based on the current contract value with the option

**Late/ Early Plan

Contract Costs (\$000) as of 03/31/10

	CM019	F/A Support*	Total
Budget at Award	756,020		756,020
Current Budget	779,859		779,859
Original Award	499,987		499,987
Approved Changes	28,379		28,379
Current Contract Value	528,366		528,366
Invoiced Amount	202,145		202,145
Estimate at Completion	779,859		779,859

* Force Account costs are reported on the FMM19 summary page.

Contract Modifications: As of 03/31/10, eleven modifications, for a total of \$28,379,684 have been executed. This period MTA executed \$18,734,022 (scope transfer) for 38th St. Vent Plant (CM013B) Part 1A.

Critical Issues:

- Continue to develop strategies to minimize impact to MNR operations and Con-Ed steam pipes during blasting.
- Continue to work through design of Wellway#1 underpinning beam so as not to impact reframing.

Quarterly Review Report – 1st Quarter '10

Coordination Required

Real Estate: All properties and easements are secured.

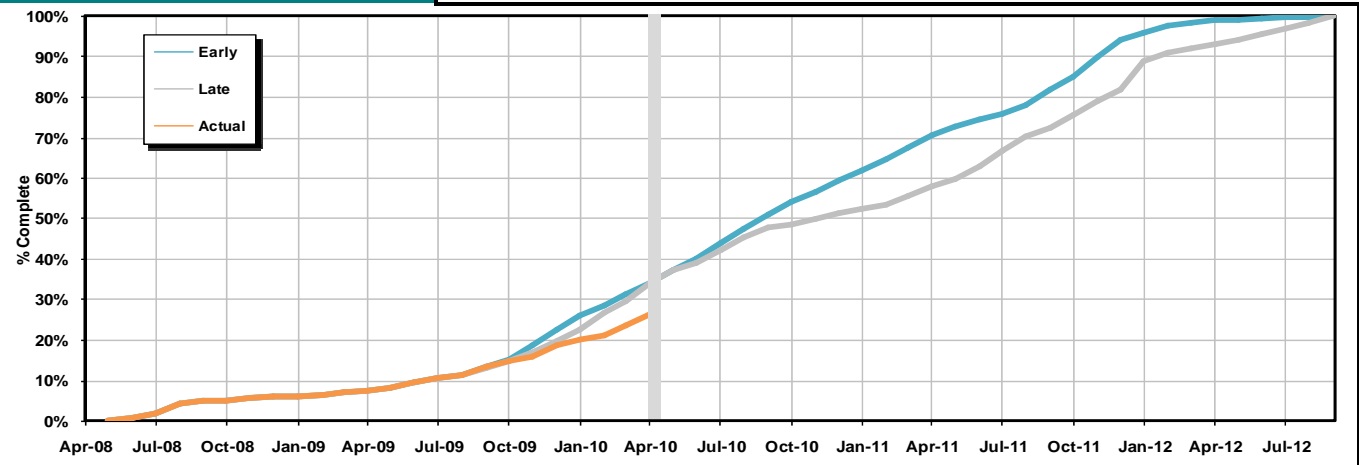
Railroad Agencies: Flagging and access and protection are being provided by MNR and NYCT.

MOU: N/A.

Permits: The contractor is responsible for securing the following:

- Blasting Permit – FDNY: Permit received.
- Lane Closure Permits – NYCDOT: Permit received for Park Ave utility work.

Progress (CM019)



Safety

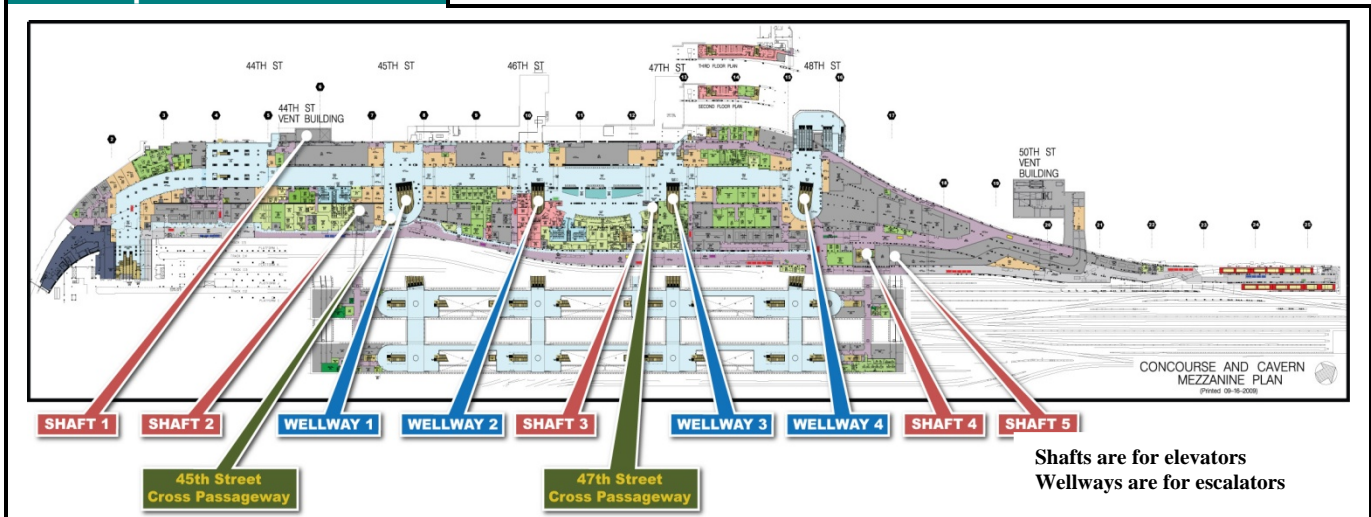
- Ongoing safety walk-thru of sites continues
- The contractor continues to maintain appropriate ventilation and housekeeping

Safety Statistics* (Cumulative Incidents per 200,000 work-hours)			
	Project	Contract	BLS**('07)
Lost Time Accidents	1.92	1.12	2.60

*The project performance statistics are based on construction hours only

**Bureau of Labor Statistics/National Industry Average

Site Map



CM008A – Madison Yard Site Clearance

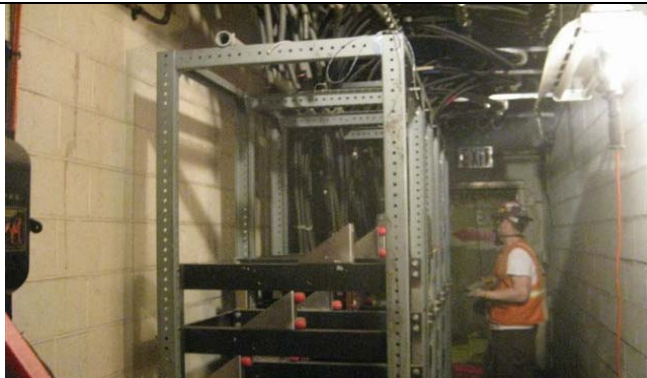
Jan, Feb, Mar '10

Contractor: Gramercy Group, Inc.
Construction Manager: Daniel Connor

FTA Grant: ARRA Stimulus Fund NY-36-0002

Description: Dismantle rail, ballast, concrete slab, crash walls, two platforms, and miscellaneous buildings; Demolish elevators and remove accompanying mechanical/electrical equipment; Remove 45th St. cross passage walls and floor for Shaft 2; Relocate 46th St. out of well way 2 excavation footprint; Plumbing: Above and below slab pipe removals and relocations and new pipe installations, HVAC: Duct removal and installation and fan work, and all other mechanical/electrical systems. Construct new MNR Commissary.

Construction



Demolition at Vanderbilt Power House.



Sawcutting and Excavation at West Perimeter Wall.

Schedule and Cost

Progress:

- Continued installation of temporary/interim drainage and overhead permanent drainage
- Completed placement of concrete slab over 45th St. cross passage
- Completed footings, slab, related utilities and commenced masonry work at commissary
- Continued concrete placement for grade beams and masonry work at the East Perimeter Wall
- Completed demolition of utilities at 45th St. and continued installation of new utilities
- Began demolition of 52 Vanderbilt Power House
- Began sawcutting and excavation for West Perimeter Wall

Schedule: The contractual intent of Milestones 2 and 3 have been met and the follow-on contractor is occupying the area and is working as planned. Some miscellaneous construction activities were not completed within the contractual deadlines established for Milestones 2 and 3. The contractor has requested those activities be shifted out of the milestone description. This request was approved by issuing a Contract Modification that modified the scope of work of Milestones 2, 3 and 4.

The contractor is in full production and has submitted a recovery schedule to meet the contractual milestones. It shows Commissary Zone 2 & 3 finishing in August '10 and is currently under review.

Cost: Project is within budget.

Critical Issues: None

Contract Modifications: As of 03/31/10, twenty modifications, for a total of \$745,104 have been executed.

* Milestones CM008A		
Milestones Description	Current Approved Plan	Actual/Forecast
Advertise (IFB)	08/25/08	08/25/08 (A)
NTP	03/09/09	03/09/09 (A)
Substantial Completion	06/2/10	8/27/10
Projected Completion of Commissary-Zone 2&3	06/2/10	08/24/10
Close-Out	08/31/10	11/14/10

Schedule Progress	Current Period		Cumulative	
Cost Expenditure	Plan*	Actual	Plan*	Actual
Percent Complete	15.4/15.8	11.4	79.7/93.7	71.9

*Late/ Early Plan

Contract Costs (\$000) as of 03/31/10			
	CM008A	F/A Support*	Total
Budget at Award	40,932		40,932
Current Budget	40,932		40,932
Original Award	38,983		38,983
Approved Changes	745		745
Current Contract Value	39,728		39,728
Invoiced Amount	28,530		28,530
Estimate at Completion	40,932		40,932

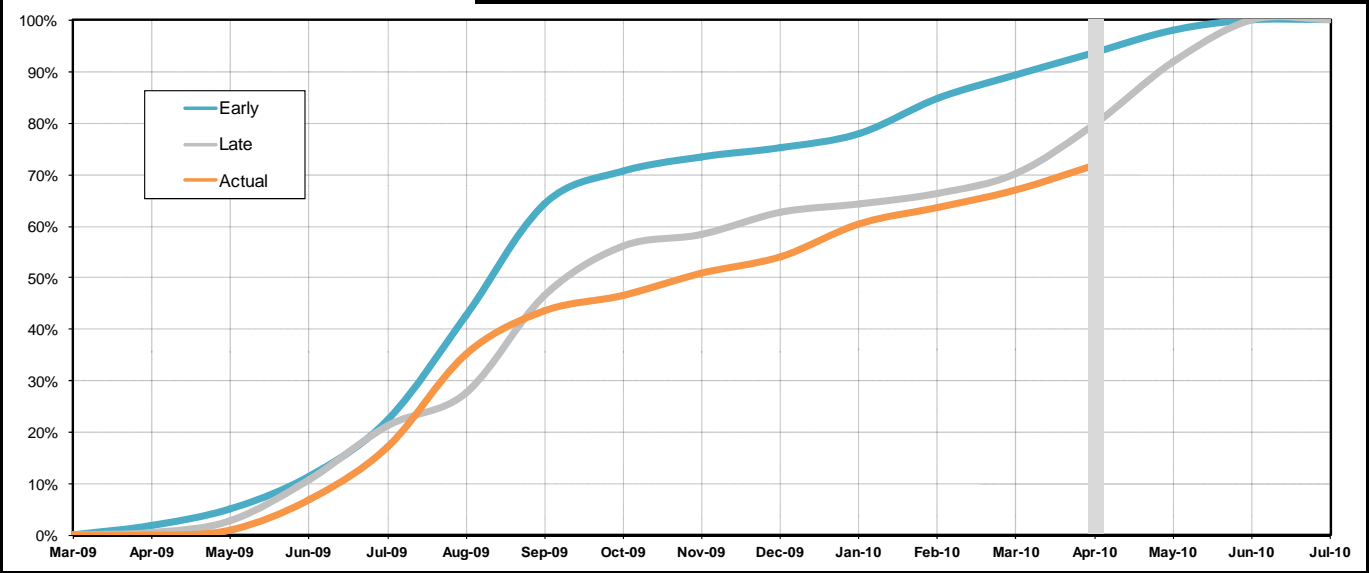
*Force Account costs are reported on the FMM19 summary page.

Quarterly Review Report – 1st Quarter '10

Coordination Issues

Real Estate: N/A
Railroad Agencies: MNR
MOU:
Permits: Demolition permit received. Construction permit received

Progress (CM008A)



Safety

- Zero Lost Time Accidents
- Continued ongoing safety walk-thru of sites
- The contractor continues to maintain appropriate ventilation and housekeeping during demolition

Safety Statistics* (Cumulative Incidents per 200,000 work-hours)			
	Project	Contract	BLS**('07)
Lost Time Accidents	1.92	0.00	2.60

*The project performance statistics are based on construction hours only
 **Bureau of Labor Statistics/National Industry Average

Site Map



CM004 – 44th St. Demolition & Construct Fan Plant Structure & 245 Park Ave. Entrance

Jan, Feb, Mar '10

Construction

Contractor: Yonkers Contracting Company, Inc.
Construction Manager: Daniel Connor

FTA Grant: ARRA Stimulus Funds NY-36-0002

Description: 47 E. 44th Street: Remove hazardous material, demolish existing building, perform soil and rock excavation. Construct shell of new building. Frame and partially fit out new building. 245 Park Avenue Entrance: Conduct environmental abatement and demolition; construct new foundations, walls and supports, furnish and install escalator. Install architectural finishes, plumbing, fire protection, HVAC, lighting, power, fire alarm, and communication systems.



Sidewalk Bridge Completed 47 East 44th Street.



Demolition of Headhouse North of platform at East 245 Park Ave.

Schedule and Cost

Progress: 44th Street Vent Plant

1. Completed abatement in floors 1, 2 and basement
2. Installed sidewalk shed and Jersey barriers along 44th St.
3. Continued installation of noise & vibration monitoring equipment

Progress: 245 Park Avenue Entrance

1. Continued to form openings between the MNR lunch room and service plant for new steel installation
2. Completed casting of footings in MNR lunch room & service plant room
3. MNR F/A: Utility relocations/removals on platform E ongoing
4. Continued demolition of north end of platform E (Tracks 11 & 13)
5. Commenced preparations for demolition at 245 Park Avenue Entrance lobby

Schedule: Baseline schedule was accepted. Contractor's latest schedule update submittal shows that the contract is on schedule and will finish on time.

Cost: Budget reflects the award amount.

Contract Modifications: As of 03/31/10, one modification for zero dollars has been executed.

Critical Issues:

1. Additional asbestos findings delayed commencing abatement. Contractor is accelerating the abatement to remain on schedule.
2. Compliance with Buy America requirements for the required elevator. Contractor's information is under review by MTA Legal.
3. Re-design of 245 Park Ave. security & communications systems is ongoing by GEC to update systems design to meet requirements

Milestones CM004

Milestone Description	Current Approved Plan	Actual/ Forecast
Advertise (IFB)	12/11/08	12/11/08 (A)
NTP	09/16/09	09/16/09 (A)
Substantial Completion	09/19/11	09/19/11
Close-Out	12/14/11	12/14/11
Schedule Progress	1st Qtr	
Cost Expenditure	Plan*	Actual
Percent Complete	5.4/9.1	5.1
	Plan*	Actual
	6.4/10.8	6.3

*Late/ Early Plan

Contract Costs (\$000) as of 03/31/10

	CM004	FA Support*	Total
Budget at Award	42,803		42,803
Current Budget	42,803		42,803
Original Award	40,765		40,765
Approved Changes	-		
Current Contract Value	40,765		40,765
Invoiced Amount	2,550		2,550
Estimate at Completion	42,803		42,803

* Force Account costs are reported on the FMM19 summary page.

Quarterly Review Report – 1st Quarter '10

Coordination Required

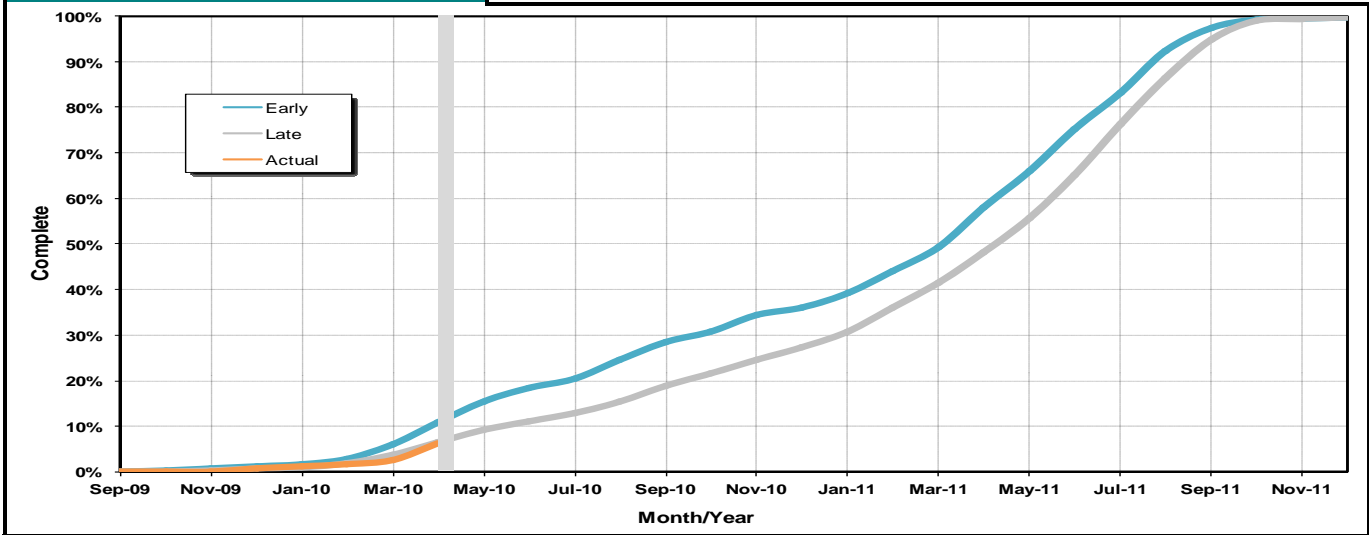
Real Estate: 47 E.44th St. acquired and vacated; NYC Planning Commission approved 245 Park Ave. change of use in Plaza area.

Railroad Agencies: MNR and LIRR.

Permits: Contractor is acquiring demolition, sidewalk and construction permits. MPT (lane closing) permits were issued by DOT.

MOU: MOU with MNR executed.

Progress (CM004)



Safety

- Zero lost time accidents
- Continued ongoing safety walk-thru of sites

Safety Statistics* (Cumulative Incidents per 200,000 work-hours)			
	Project	Contract	BLS**('07)
Lost Time Accidents	1.92	0.00	2.60

*The project performance statistics are based on construction hours only
 **Bureau of Labor Statistics/National Industry Average

Site Map



CM013 – 50th St. Vent Facility

Jan, Feb, Mar '10

Contractor: CCA Civil-Halmar International
Construction Manager: Vincent Monaco

FTA Grant: ARRA Stimulus Funds NY-36-0002

Construction

Description: Construct 50th St. Ventilation Facility. Work includes: building demolition including environmental abatement, termination of utility connections; foundation excavation and lining; shaft and tunnel excavation and lining; and construction of new ventilation building structure and vertical utility shaft



Trench Excavation for Utility Relocation on 50th St.



Cladding Installation at 300 Park Ave.

Schedule and Cost

Progress:

- Final Detailed Cost Breakdown (DCB) issued by contractor and Resource Loaded Baseline Schedule is under review
- Began excavation of trenches for gas and steam utility replacement on 50th St.
- Monitoring instrumentation installed in buildings close to the construction area
- On-going installation of cladding of north and west face of 300 Park Ave. and building's temporary inside wall prior to the demolition of adjacent buildings
- 437 Madison Ave building continues building of internal temporary walls prior to the demolition of adjacent buildings

Schedule: Final baseline schedule will be submitted by the contractor in early April '10

Cost: Budget reflects the award amount.

Critical Issues: None.

Milestones CM013

Milestone Description	Current Approved Plan		Actual/Forecast	
	Start	End	Start	End
Advertise (IFB)	07/09		07/09 (A)	
NTP	01/04/10		01/04/10 (A)	
300 Park Ave Independent Wall and Prepare Site Access	12/15/10		12/15/10	
Installation of Street Decking	11/17/10		11/17/10	
Substantial Completion	06/01/12		06/01/12	
Close-Out	09/04/12		09/04/12	
	1st Qtr		Cumulative	
Cost Expenditure	Plan*	Actual	Plan*	Actual
Percent Complete	1.0/3.2	1.5	1.0/3.2	1.5

*Late/ Early Plan

Contract Costs (\$000) as of 03/31/10

	CM013	F/A Support	Total
Budget at Award	99,073	878	99,951
Current Budget	99,073	878	99,951
Original Award	94,355	878	95,233
Approved Changes	0	0	0
Current Contract Value	94,355	878	95,233
Invoiced Amount	1,454	0	1,454
Estimate at Completion	99,073	878	99,951

* Force Account costs are reported with CM013.

Quarterly Review Report – 1st Quarter '10

Coordination Required

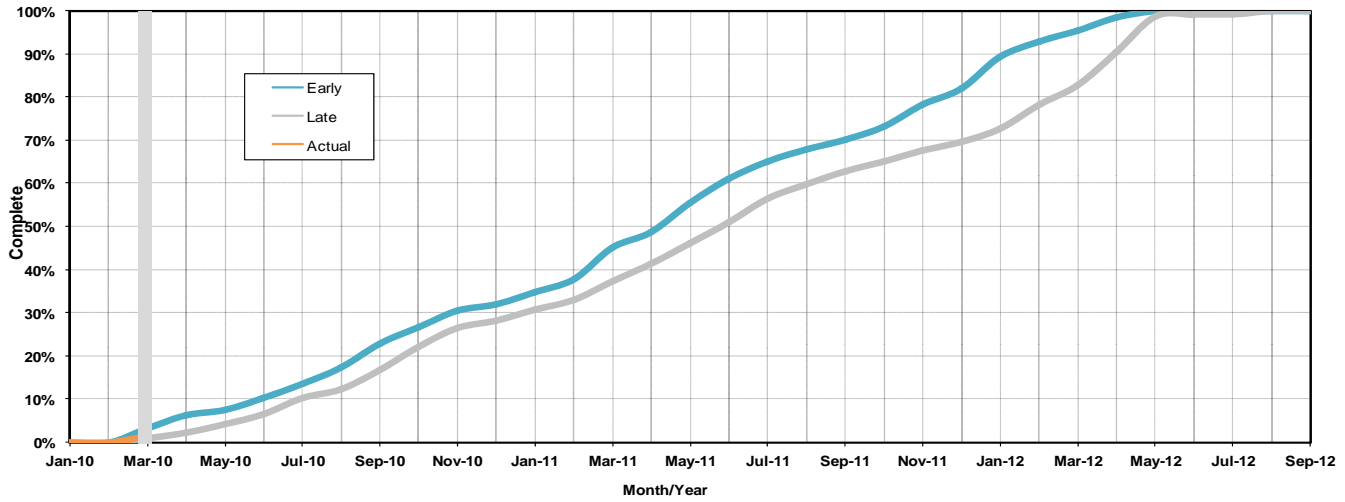
Real Estate: All properties and easements are secured

Railroad Agencies: MNR

MOU: NYCDOT MOA executed, 437 Madison MOU executed, 300 Park MOU executed, NYCDEP expected execution in early April

Permits: The contractor is responsible for securing the following:
 Blasting Permit – FDNY, Lane Closure Permits – NYCDOT,
 Building Permits – NYC Buildings and Asbestos permits

Progress (CM013)



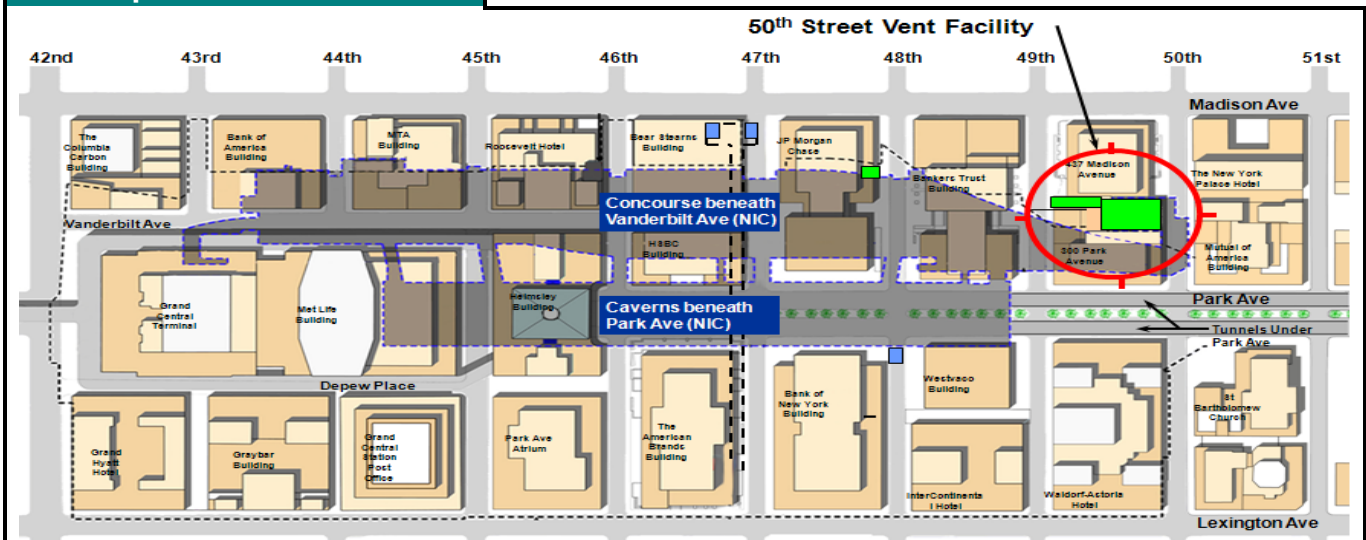
Safety

- Zero lost time accidents
- Continued ongoing safety walk-thru of sites

Safety Statistics* (Cumulative Incidents per 200,000 work-hours)			
	Project	Contract	BLS**('07)
Lost Time Accidents	1.92	0	2.60

*The project performance statistics are based on construction hours only
 **Bureau of Labor Statistics/National Industry Average

Site Map



Quarterly Review Report – 1st Quarter '10

FM216 – MNR Traction Power MODs & 13.2 kV Loop

Jan, Feb, Mar '10

Construction

Contractor: TC Electric
Construction Manager: James Murray
FTA Grant: NY-03-0344

Description: Furnish, deliver and install automated system for motor-operated disconnect switches (MODs). Scope of work also includes reconfiguration of 13.2kV Traction Power feeder cables.



New Switchboard SB-72 in Switchboard Room D.



Cable pull for new 13.2KV feeder at Platform U.

Schedule and Cost

Progress:

1. Completed the relocation of the east side Conduit Rack #62 from east of the UA Wall to track 115
2. Completed pulling cables for negatives feeder 703A & B, energized new feeders by MNR and removed old feeders in the way of steel erection at escalator Wellways 1 & 2
3. Completed the cut-over of 13.2 kV feeder between TH1-TH3
4. Continued installation of cables for power and control feeders for transformer houses
5. Continued the installation of conduit runs for negative feeders (96% completed) and positive feeders (92% completed) in the upper level and started pulling cables
6. Continued the replacement of existing switchboards in Switchboard Room D (95% completed)
7. Continued installation of Motor Operated Disconnect Switches and associated conduit/cable (50% completed)

Schedule: Contract is behind schedule due to difficulty in securing track outages and flaggers. Since the supplemental coordination personnel arrived in Oct '09, track outage availability increased. This support will help recover the schedule.

In an effort to expedite installation of the MODs and their associated conduit runs, MNR agreed to track outages that span multiple tracks on the East side of lower level, starting March 1st. This should help the contractor to complete MOD installation by the revised substantial completion date of June 30, 2010. MNR Force Account cut-over of MODs may not occur until after Substantial Completion.

Milestone dates, progress curve and percentages have been modified to correlate with a contract modification that deleted/revised milestones and revised the substantial completion dates.

Cost: Project is within budget. Current budget includes scope transfer of \$272,042 for Traction Power MODs which originally was to be performed by MNR under *Manhattan Structures MNR (FMM19 contract)*. Scope transfer \$1,310,009 to relocate ductbank #62 originally part of *Harold Structures Part 3(CH058)* scope to be approved in April.

Milestones FM216				
<i>Milestone Description</i>	<i>Current Approved Plan</i>		<i>Actual/Forecast</i>	
Advertise (IFB)	05/08		05/08(A)	
NTP	09/25/08		09/25/08 (A)	
Complete all Installation work at Escalator Wellways 1, 2 and 3	03/10		03/10 (A)	
Substantial Completion	06/30/10		06/30/10	
Close-Out	09/28/10		09/28/10	
Schedule Progress	<i>1st Qtr</i>		<i>Cumulative</i>	
<i>Cost Expenditure</i>	<i>Plan*</i>	<i>Actual</i>	<i>Plan*</i>	<i>Actual</i>
Percent Complete	16.5/19.7	25.0	85.3/91.7	71.3

**Late/ Early Plan*

Contract Costs (\$000) as of 03/31/10			
	<i>FM216</i>	<i>MNR Share</i>	<i>Total</i>
Budget at Award	14,479	4,348	18,827
Current Budget	16,075	4,348	20,423
Original Award	13,790	4,140	17,930
Approved Changes	344	0	344
Current Contract Value	14,133	4,140	18,273
Invoiced Amount	11,977	2,149	14,126
Estimate at Completion	16,075	4,348	20,423

** Force Account costs are reported on the FMM19 summary page.*

Contract Modifications: As of 03/31/10, three modifications, for a total of \$343,930 have been executed. A contract modification is currently being processed for revision of contract milestones.

Critical Issues: Keeping installation performance moving forward by coordinating with MNR to piggyback on outages required for their operational needs and other ESA contracts.

Coordination Issues

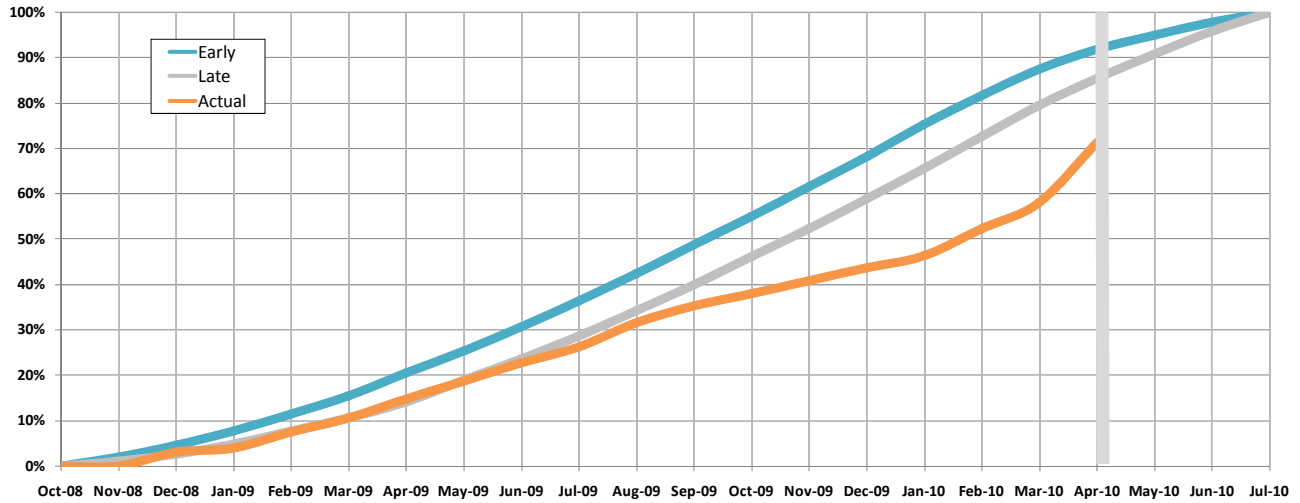
Real Estate: N/A

MOU: N/A

Railroad Agencies: MNR

Permits: N/A

Progress (FM216)



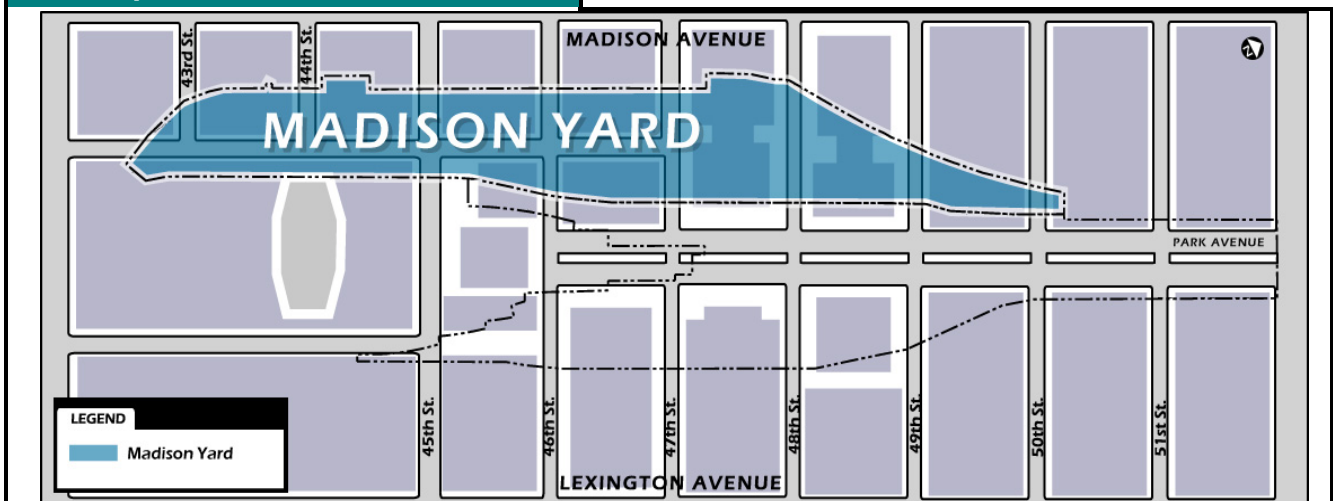
Safety

- Continue ongoing safety walk-thru of sites.
- The contractor continues training while maintaining area vigilance and housekeeping

Safety Statistics* (Cumulative Incidents per 200,000 work-hours)			
	Project	Contract	BLS**('07)
Lost Time Accidents	1.92	4.36	2.60

*The project performance statistics are based on construction hours only
 **Bureau of Labor Statistics/National Industry Average

Site Map



CS790 – GCT Protection Works

Jan, Feb, Mar '10

Contractor: Ad-Tech Enterprises
Construction Manager: Larry Nigro

FTA Grant: N/A

Construction

Description: Make all necessary concrete repairs to roof slabs, columns and beams and install protective netting in GCT from 42nd thru 51 Streets (Upper Level Tracks 32 through 42, Lower Level Tracks 113 through 115).



Net Installation at Platform T.



Net Installation at Platform R.

Schedule and Cost

Progress:

1. Completed Asbestos Containing Materials (ACM) pipe insulation abatement in the Upper Level
2. Completed netting installation at Shaft 3 blast zone
3. Completed 50% of netting installation at switches north of Gridline 15
4. Completed netting installation at Shafts 4 & 5 Blast Zone
5. Completed 75% of netting installation at Shaft 2 Blast Zone
6. Completed 65% of concrete repairs
7. Completed 60% of brackets and cables for netting installation
8. Completed 50% of netting installation

Schedule: Contractors schedule has been adjusted to support blasting operations.

Cost: The budget was increased to reflect an expected future change order to re-insulate an abated drain line.

Contract Modifications: As of 03/31/10, four modifications for a total of \$ 1,630,948 have been executed.

Safety: No recorded lost time incidents.

Critical Issues: Coordination with MNR on outages at Upper Level for netting system installation controls progress.

Milestones CS790		
Milestones Description	Current Approved Plan	Actual/Forecast
Advertise (RFP)	06/08	08/08 (A)
NTP	10/22/08	10/22/08 (A)
Substantial Completion	06/10/10	07/28/10
Close-Out	7/10	7/10

Schedule Progress	1st Qtr		Cumulative	
Cost Expenditure	Plan	Actual	Plan	Actual
Percent Complete	5.0	4.0	90.0	77.0

Contract Costs (\$000) as of 03/31/10			
	CS790	F/A Support*	Total
Budget at Award	12,841	N/A	12,841
Current Budget	12,960	N/A	12,960
Original Award	10,329	N/A	10,329
Approved Changes	1,631	N/A	1,631
Current Contract Value	11,960	N/A	11,960
Invoiced Amount	8,261	N/A	8,261
Estimate at Completion	12,960	N/A	12,960

* Force Account costs are reported on the FMM19 summary page.

Coordination Required

Real Estate: N/A

Railroad Agencies: MNR

Permits: Issued

MOU: This contract is covered by existing MNR MOU.

Queens/Harold Active Contracts

Queens Instrumentation (CS810) – Procurement, installation and establishment of initial readings for structural instrumentation, geotechnical instrumentation, and data acquisition systems to support Queens tunnel excavation.

Queens Bored Tunnels (CQ031) – Construct three revenue tunnels and one non-revenue tunnel beneath Sunnyside Yard and Harold Interlocking.

Northern Boulevard Crossing (CQ039) – Construct mined tunnel under Northern Boulevard, the IND underground subway and the BMT elevated subway.

Harold Structures 1 and GO2 Substation (CH053) – Construct various civil infrastructure elements at Harold Interlocking and expand the existing LIRR Amtrak Right of Way for the future TBM tunnels and Mainline track diversions including the design, and installation of a pre-fabricated substation.

Harold Structures 2 (CH054A) – Construct various civil infrastructure elements at Harold Interlocking and expand the existing LIRR and Amtrak Right of Way for the future TBM tunnels and Main-line track diversions.

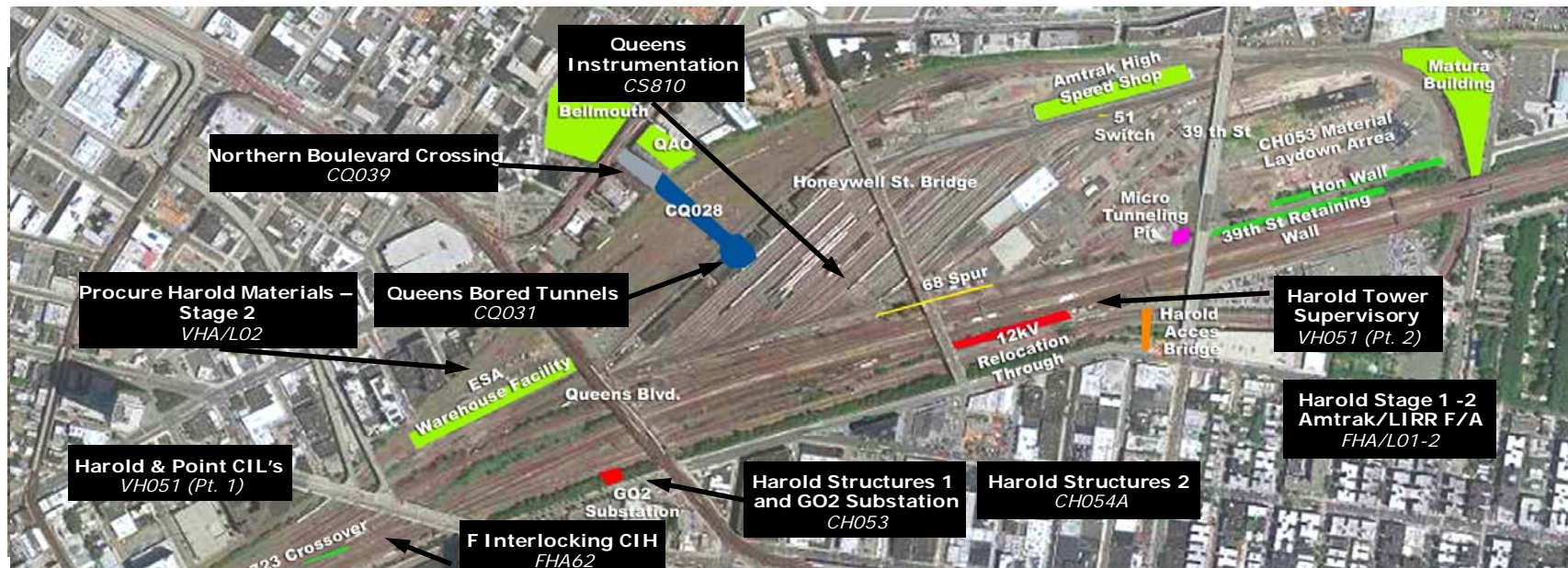
Harold Stage 1-2 Amtrak/LIRR Force Account (FHA/L01-2) – Installation of track/switches by LIRR and Amtrak. Installation of new microprocessor based signal system. Construction of traction power substation. Modified catenary system.

F Interlocking CIH (FHA62) – Amtrak manufacture and delivery of Central Instrumentation Houses (CIHs) and cases for the F2 and F1 Signal Interlockings.

Procure Harold Materials Stage 2 (VHA/L02) – Procure LIRR and Amtrak Force Account material in support of Stage 2 construction.

Harold & Point CIL's (VH051 Part 1) – This is the procurement of seven Harold Interlocking Central Instrument Locations (CILs) and one Point Interlocking Central Instrument Location and the purchase of a fully operational Vital Microprocessor-Based Interlocking Control System for the referenced interlockings.

Harold Tower Supervisory (VH051 Part 2) – This procurement contract requires purchase of a fully operational Supervisory Control System for control of signals at Harold and Point Interlocks.



Active Projects – Queens

CS810 – Queens Instrumentation

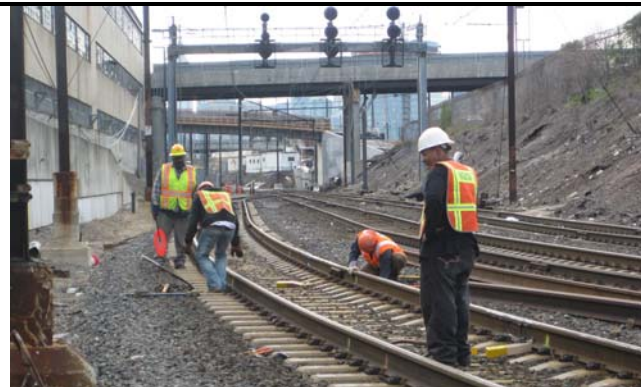
Jan, Feb, Mar '10

Contractor: Wang Technology, LLC
 Construction Manager: John Annoni

FTA Grant: N/A

Construction

Description: Procurement, installation and establishment of initial readings for structural instrumentation, geotechnical instrumentation, and data acquisition systems in Sunnyside Yard and Harold Interlocking to support Queens tunnel excavation. The instrumentation will monitor on existing ground and structure settlement during the TBM operation.



Upgrading Track Dynamic Monitoring Points at 43rd St. Bridge.



Drilling Piezometers at Sunnyside Yard.

Schedule and Cost

Progress:

1. Completed installation of structural monitoring points under 43rd St. Bridge for baseline readings
2. Completed drilling and grouting borehole extensometer and started drilling piezometer
3. Continued upgrading track dynamic profile monitoring points in Sunnyside Yard and Loop tracks
4. Tested Automatic Motorized Total Stations (AMTS) installed at existing catenary structures prior to installation at final locations

Schedule: Not able to complete the work as planned due to deployment of the railroad resources to other contracts based on overall program priority. A revised schedule has been submitted accordingly. An adjustment to the scope of work will be made to facilitate the current TBM schedule.

Cost: Costs are within budget. The budget will be reduced to reflect the deletion of liquid level instrumentation under Change Proposal Request #1 and it is expected that the contract will be completed within or below budget.

Contract Modifications: As of 03/31/10, one modification has been executed with no dollar value.

Critical Issues: Limitation in Amtrak Electrical Traction resources delays the planned drilling schedule.

Milestones CS810

Milestones Description	Current Approved Plan	Actual/ Forecast
NTP	05/06/09	05/06/09 (A)
Complete prisms at 43 rd St Bridge	09/01/09	04/01/10
Instrumentation at Yard lead area	12/01/09	04/14/10
Instrumentation near three tunnel exit	02/04/10	05/12/10
Substantial Completion	05/05/10	06/23/10

Schedule Progress	1st Qtr		Cumulative	
	Plan*	Actual	Plan*	Actual
Cost Expenditure				
Percent Complete	20.3/26.7	0.7	60.9/82.4	44.8

*Late/ Early Plan

Contract Costs (\$000) as of 03/31/10

	CS810	F/A Support*	Total
Budget at Award	3,007	-	3,007
Current Budget	2,737	-	2,737
Original Award	2,737	-	2,737
Approved Changes	-	-	-
Current Contract Value	2,737	-	2,737
Invoiced Amount	1,240	-	1,240
Estimate at Completion	2,737	-	2,737

* Force Account Access and protection costs are reported with CH053.

CQ031 – Queens Bored Tunnels and Structures

Jan, Feb, Mar '10

Contractor: Granite-Traylor-Frontier Joint Venture
Construction Manager: Mike Piepenburg

FTA Grant: NY-03-0344/ARRA Stimulus Funds NY-36-0002

Construction

Description: Construct three revenue tunnels and one non-revenue tunnel beneath Sunnyside Yard and Harold Interlocking. Construction includes the following; complete main open-cut excavation (from CQ028), procurement and assembly of two pressurized face tunnel-boring machines and ancillary equipment, tunnel excavation, installation of a one-pass pre-cast concrete tunnel liner, installation of ground treatment along the alignment to support tunnel construction, underpinning bridge piers and LIRR mainline structures, construction of an Amtrak access road, and construction of exit/ventilation structures (below-ground) and TBM reception pit, environmental monitoring, water quality sampling and elevation.



Installing the Vehicular Bridge near Northern Blvd.



Installing Conduits at Temporary Substation.

Schedule and Cost

Progress:

1. Completed demolition of Amtrak buildings 1, 2, 5 and 6 in Sunnyside Yard. Continued removing debris. Completed ground improvement Zone 2. Began sewer and water main relocation at Q-tip area.
2. Completed the slurry wall repairs. Dewatered residual water. Installed the vehicular steel bridge across the part of the open-cut. Commenced mass excavation at the part of the open-cut.
3. Installed electrical conduits and manholes for and began construction of the on-site electrical for TBM substation
4. Installed test piles and began installation of mini-piles at East abutment for 43rd St underpinning
5. Continued environmental monitoring, water quality sample and elevation as part of Long Island Well Permit's report (transferred from contract CQE28-02)
6. Procurement of TBM is on schedule

Schedule: The baseline schedule is conditionally approved. The recovery plan for open-cut excavation is being submitted.

Cost: As of 03/31/10, four modifications for a total of \$58,404,095 have been executed. This includes the option work of \$58,400,000 million for the Construction of Yard Lead Approach and Other Structures.

Critical Issues:

1. Complete open-cut excavation which controls the critical path of the tunnel mining.
2. Coordinate limited Amtrak ET resources with adjacent contractors
3. The recovery schedule will increase number of shifts per day for the open-cut excavation provided by the contractor. The current work for the open-cut excavation is on schedule.

Milestones CQ031

Milestones Description	Current Approved Plan	Actual/Forecast
NTP	09/28/09	09/28/09 (A)
Temporary Substation for TBM	09/27/10	09/28/10
Complete Tunnels	03/04/12	03/08/12
Substantial Completion	09/26/12	09/28/12
Close-Out	01/24/13	01/24/13

Schedule Progress	1st Qtr		Cumulative	
	Plan*	Actual	Plan*	Actual
Cost Expenditure				
Percent Complete	11.1/11.4	7.5	16.5/23.5	11.8

*Late/ Early Plan

Contract Costs (\$000) as of 03/31/10

	CQ031	F/A Support	Total
Budget at Award	756,763	22,677	779,440
Current Budget	756,763	22,677	779,440
Original Award	659,201	9,825	669,026
Approved Changes	58,404	0	58,404
Current Contract Value	717,605	9,825	727,430
Invoiced Amount	84,990	1,625	86,615
Estimate at Completion	756,763	22,677	779,440

Quarterly Review Report – 1st Quarter '10

Coordination Required

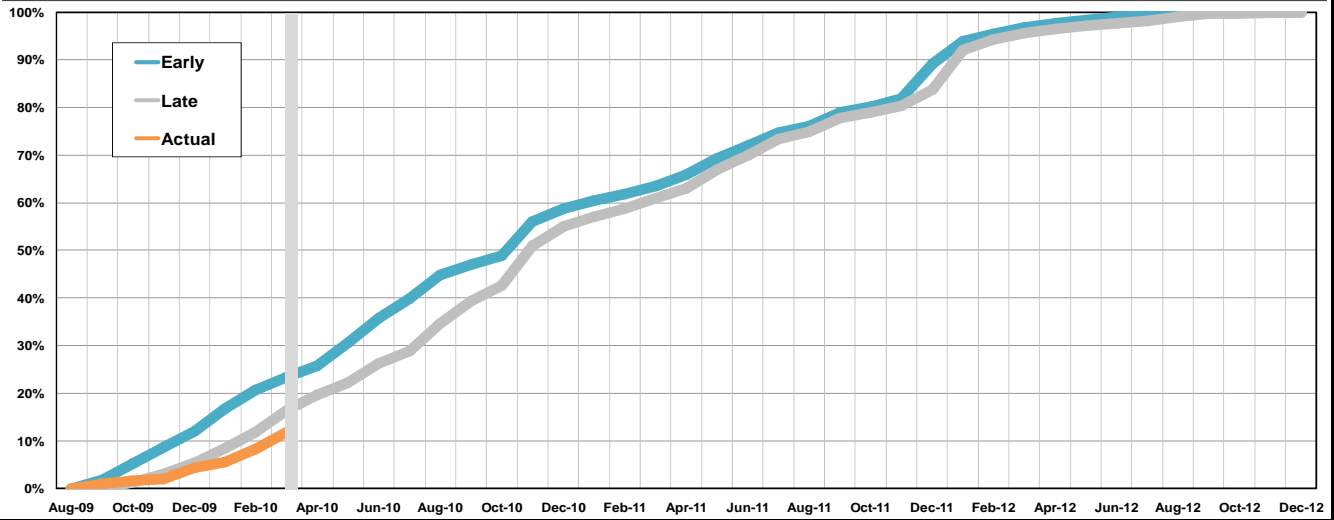
Real Estate: All Real Estate has been acquired and tenants are being relocated.

Permits: A FDNY blasting permit and a water discharge permit are required. Long Island well permit has been obtained (from CQ028).

Railroad Agencies: Amtrak, LIRR, NYAR.

MOU: Amtrak Project Initiative and Permit to Enter obtained.

Progress Curve (CQ031)



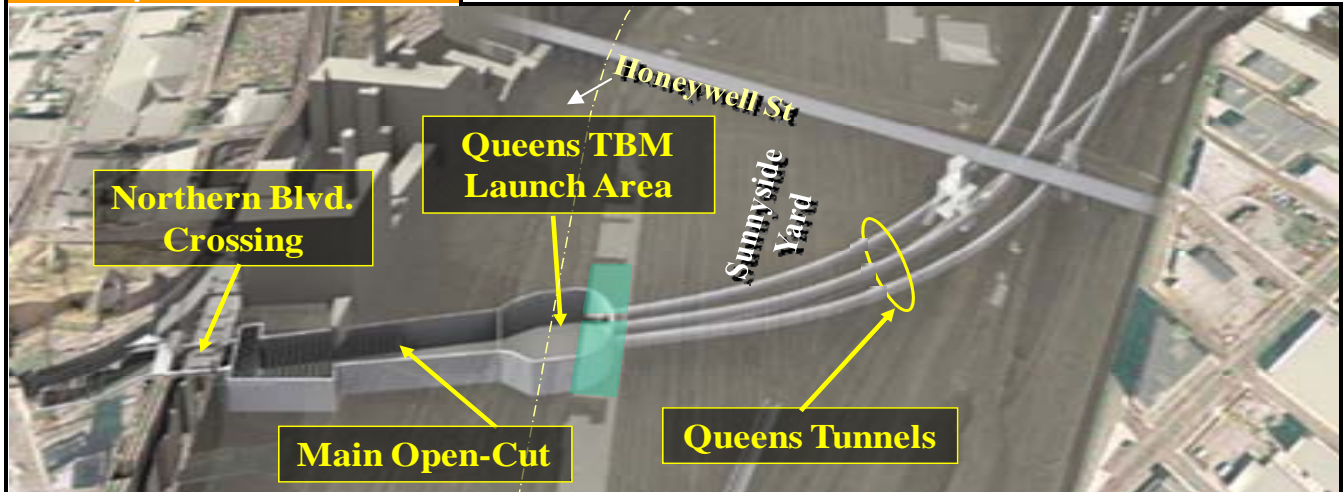
Safety

- Continue ongoing safety walk-thru of sites

Safety Statistics*			
<i>(Cumulative Incidents per 200,000 work-hours)</i>			
	Project	Contract	BLS**('07)
Lost Time Accidents	1.92	1.77	2.60

*The project performance statistics are based on construction hours only
 **Bureau of Labor Statistics/National Industry Average

Site Map



CO039 – Northern Boulevard Crossing

Jan, Feb, Mar '10

Contractor: Schiavone/ Kiewit, a Joint Venture
Construction Manager: Philip Stummvoll

FTA Grant: NY-03-0344

Construction

Description: Construct mined tunnel under Northern Boulevard, the IND underground subway and the BMT elevated subway. The work includes: 1) Ground improvement/freezing; 2) Underpinning BMT foundations; 3) Complete excavation and support existing slurry walls; 4) Geotechnical instrumentation/monitoring; 5) Sequential tunnel excavation and initial support; and 6) Final structural lining and invert.



Trench Excavation for Installation of Temporary Utility Lines.



Site Walk Closure and Fencing for Slurry Wall Repair.

Schedule and Cost

Progress:

1. Notice to Proceed was issued February 3rd, 2010 and a kick-off meeting was held
2. Started site mobilization, office trailers set-ups and site clean-up. Installation of temporary utilities continued.
3. Conducted construction quality, safety and code compliance meetings. Received initial submittals of which 11 were returned to the contractor and 10 remain under review.
4. Began site walk closure for slurry wall repair around Early Access Chamber

Schedule: The baseline schedule was submitted 2/16/10 and CM comments were returned 3/4/10. Contractor is currently incorporating comments to the baseline schedule.

Cost: See Contract costs for current budget.

Critical Issues: None

Milestones CQ039				
Milestones Description	Current Approved Plan	Actual/ Forecast		
NTP	02/03/10	02/03/10 (A)		
Substantial Completion	10/05/11	10/05/11		
Close-Out	02/04/12	02/04/12		
Schedule Progress	1st Qtr		Cumulative	
Cost Expenditure	Plan	Actual	Plan	Actual
Percent Complete	TBD	TBD	TBD	TBD
Contract Costs (\$000) as of 03/31/10				
	CQ039	F/A Support	Total	
Budget at Award	89,245	1,245	90,490	
Current Budget	89,245	1,245	90,490	
Original Award	84,950	0	84,950	
Approved Changes	0	0	0	
Current Contract Value	84,950	0	84,950	
Invoiced Amount	0	0	0	
Estimate at Completion	89,245	1,245	90,490	

Coordination Required

Real Estate: N/A

Railroad Agencies: The preliminary design is already approved by NYCT. The final design will be performed by GEC. Instrumentation and monitoring are being coordinated with NYCT.

Permits: Lane closure (DOT) and water discharge (DEP) permits will be obtained by contractor.

MOU: N/A

Quarterly Review Report – 1st Quarter '10

CH053 – Harold Structure Part 1 and G02 Substation

Jan, Feb, Mar '10

Contractor: Perini Corp.

Construction Manager: Ray Busch

FTA Grant: NY-03-0344

Description: Construct various civil infrastructure elements at Harold Interlocking and expand the existing LIRR Amtrak Right of Way for the future TBM tunnels and Mainline track diversions including the design, and installation of a pre-fabricated substation.

Construction



Micro-Tunnel Machine Breaks Out at Observation Shaft Run 6-7.



Installed Catenary Steel Structure Subset F in Sunnyside Yard.

Schedule and Cost

Progress:

1. Installed thirteen catenary steel structures along mainline tracks east of 43rd St. and loop tracks. Installed six catenary steel structures Subset F in Sunnyside Yard
2. Completed boring microtunnel Run 8 from Jacking Pit 1 crossing under 39th St. Bridge to Receiving Pit 1. Completed a section of Run 6-7 from Jacking pit 1 to the observation shaft.
3. Completed installation of the storm drain line from Jacking Pit #3 to Receiving Pit #3 and tie-in to the existing sewer line
4. Continued Harold Access Bridge installation. Completing shield and utility line under the steel girders.
5. Continued construction of G02 substation foundation for the lower vault
6. Continued excavation, rebar and concrete for catenary pole and signal tower foundations. Fabrication of catenary structures and cross members progressed off-site.
7. Continued sheeting and installation of sewer line for Retaining Wall 43-S2

Schedule:

1. An initial delay was experienced due to late issuance of the Amtrak Project Initiative and Amtrak entry permit. Time impact has been negotiated and associated costs are under negotiation.
2. An additional delay was experienced for design of catenary and signal tower foundations and structures.
3. Contractor submitted a draft of the re-baseline schedule and it has been conditionally accepted. See forecast milestone table.

Cost: Delays accessing the work site initially, scope transfers and changes to design have impacted cost. The EAC has been revised to reflect these changes. The option work of \$2 million for the Thompson Ave. Microtunnel was exercised.

Contract Modifications: As of 03/31/10, twenty six modifications for a total of \$11,221,412 have been executed.

Milestones CH053

Milestones Description	Current Approved Plan	Actual/ Forecast
NTP	01/01/08	01/01/08 (A)
Harold Access Bridge Beneficial Occupancy	01/01/11	06/16/10
New G02 Substation Complete	04/15/11	04/26/11
Substantial Completion	01/16/12	01/16/12
Close-Out	04/15/12	04/15/12

Schedule Progress	1st Qtr		Cumulative	
Cost Expenditure	Plan*	Actual	Plan*	Actual
Percent Complete**	2.2/8.0	3.9	27.0/34.3	35.4

*Late/ Early Plan

** Re-baseline schedule is conditionally approved assuming a substantial completion date of January 2012. Substantial completion revision to Oct. 2011 is possible for next month.

Contract Costs (\$000) as of 03/31/10

	CH053	F/A Support	Total
Budget at Award	144,144	18,212	162,356
Current Budget	152,708	20,560	173,268
Original Award	137,280	20,560	157,840
Approved Changes	11,221	0	11,221
Current Contract Value**	148,501	20,560	169,061
Invoiced Amount	53,325	11,910	65,235
Estimate at Completion	179,708	20,560	200,268

** Additional \$2 million option from LIRR is not included in the Current Contract Value

Critical Issues: Resolve Amtrak ET (electrical traction) and 12KV ductbank design issues so this work can resume. Complete catenary and signal tower design packages.

Complete the tri-vent utility relocation to allow CQ031 contractor to begin work in this area.

Quarterly Review Report – 1st Quarter '10

Coordination Required

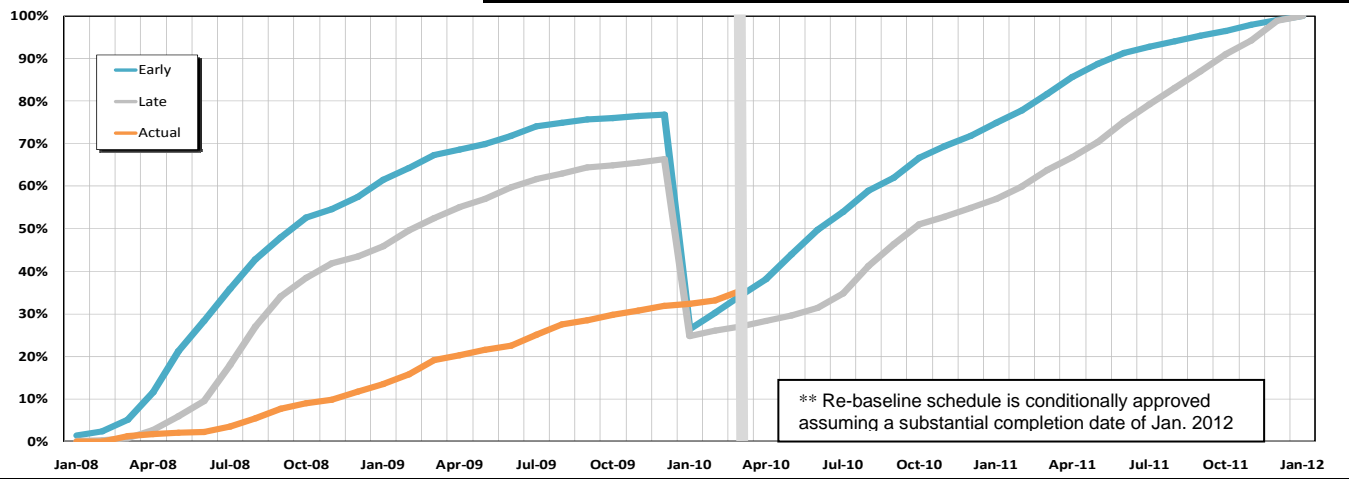
Real Estate: All easements have been secured.

Railroad Agencies: LIRR and Amtrak will provide flagging protection and perform required support work.

Permits: Pre-award reviews and approvals have been obtained from NYCDOT and NYCDEP. Relevant permit have been obtained by the contractor. Amtrak Entry Permit approved.

MOU: Amtrak labor clearance was obtained. LIRR and Amtrak force account agreements were signed. Project Initiative (PI) with Amtrak signed.

Progress Curve (CH053)



Safety

Perini, ESA and the railroads have implemented a Truck Access Management Protocol to define truck routes, staging areas and minimize hazards associated with truck movements.

Safety Statistics*			
<i>(Cumulative Incidents per 200,000 work-hours)</i>			
	Project	Contract	BLS**('07)
Lost Time Accidents	1.92	0.95	2.60

*The project performance statistics are based on construction hours only
 **Bureau of Labor Statistics/National Industry Average

Site Map



CH054A – Harold Structures – Part 2A

Jan, Feb, Mar '10

Contractor: Perini Corp.

Construction Manager: Ray Busch

FTA Grant: NY-03-0344/ARRA Stimulus Funds NY-36-0002

Description: Construct various civil infrastructure elements at Harold Interlocking and expand the existing LIRR and Amtrak Right of Way for the future TBM tunnels and Main-line track diversions including: 1) the construction of retaining walls, 2) Track D TBM reception pit, 3) AC traction power duct bank, 4) access road work, 5) demolition of existing Catenary Foundations, 6) procurement of two signal bridges, and 7) other utility relocations and adjustments.

Construction



Installation of Retaining Wall THOM-S1.



Installation of Steel Casing at Retaining Wall THOM-S2.

Schedule and Cost

Progress:

1. Completed steel casings, excavated and set soldier piles for retaining wall THOM-S1. Installation of timber lagging is in progress.
2. Began retaining wall THOM-S2 drilling steel casing and setting of soldier piles

Schedule: The baseline schedule was approved. The contractor's schedule updates were submitted. Due to weather and site conditions progress is 50% below plan. The project team is working with the contractor to assess and mitigate schedule impacts.

Cost: The budget includes funds for changes to the 12KV ductbank which is to be issued as a change order.

Contract Modifications: As of 03/31/10, one contract modification has been executed with no cost impact.

Critical Issues: Resolve Amtrak ET (electrical traction) 12KV ductbank design so changes can be negotiated and work resumed. A Change Proposal Request has been issued to contractor.

Milestones CH054A

Milestones Description	Current Approved Plan	Actual/Forecast
NTP	08/24/09	08/24/09 (A)
Retaining Wall THOM-S1 and S2	05/26/10	05/26/10
Substantial Completion	12/21/10	12/21/10
Closeout	03/21/11	03/21/11

Schedule Progress	1st Qtr		Cumulative	
	Plan	Actual	Plan	Actual
Cost Expenditure				
Percent Complete	9.0/17.6	5.9	15.6/29.5	6.8

Contract Costs (\$000) as of 03/31/10

	CH054A	F/A Support	Total
Budget at Award	27,067	3,199	30,266
Current Budget	27,067	3,199	30,266
Original Award	21,778	3,199	24,977
Approved Changes	0	0	0
Current Contract Value	21,778	3,199	24,977
Invoiced Amount	1,482	133	1,615
Estimate at Completion	27,067	3,199	30,266

Quarterly Review Report – 1st Quarter '10

Coordination Required

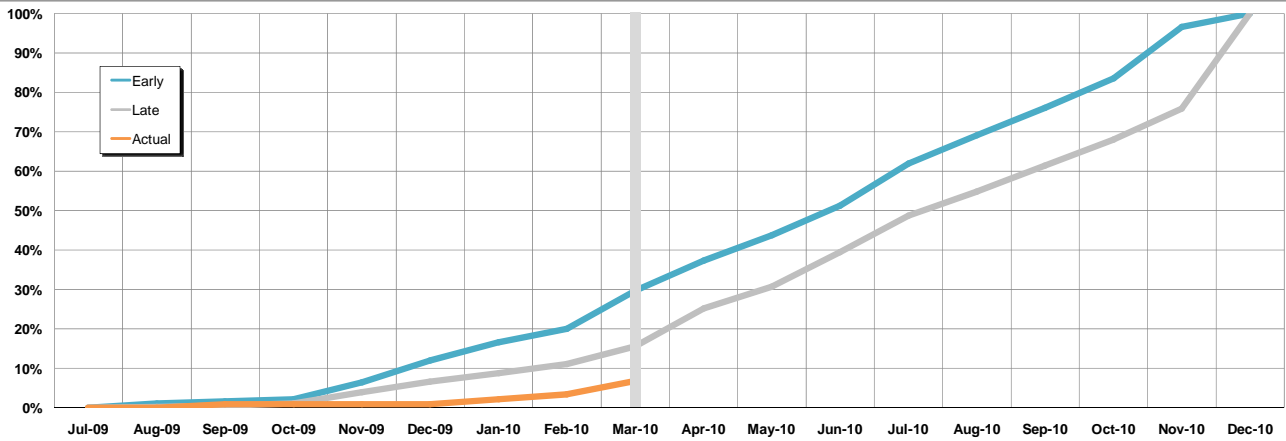
Real Estate: All work on railroad property or in city streets.

Permits: NYCDOT permits needed. Water discharge permit (DEP) needed.

Railroad Agencies: Amtrak and LIRR.

MOU: Obtained Project Initiative and Permit to Enter from Amtrak.

Progress Curve (CH054A)



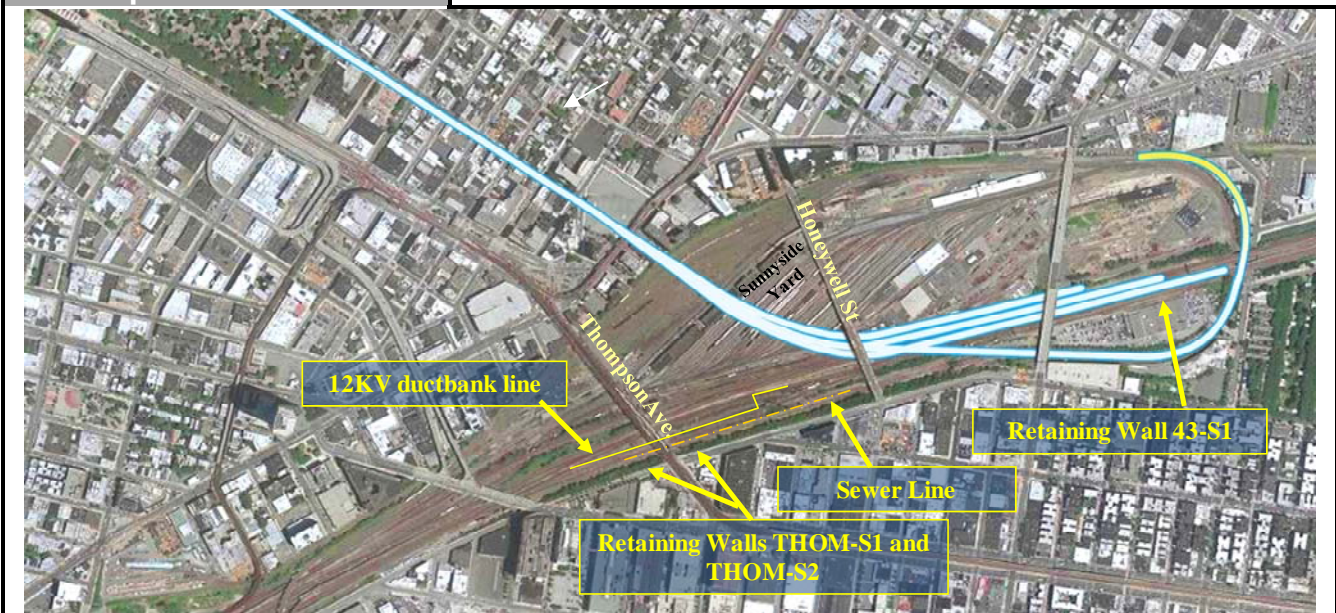
Safety

- The contract just started and a Lost Time Accident occurred. The LTA rate will decrease sharply as MTACC Safety and the contractor remains diligent with respect to safety oversight.
- Continue on-going safety walk-throughs of work sites.

Safety Statistics* (Cumulative Incidents per 200,000 work-hours)			
	Project	Contract	BLS**('07)
Lost Time Accidents	1.92	30.43	2.60

*The project performance statistics are based on construction hours only
 **Bureau of Labor Statistics/National Industry Average

Site Map



FHA01 – Harold Stage 1 – Amtrak Direct Force Account

Jan, Feb, Mar '10

Contractor: Amtrak

Construction Manager: Peter Malvese

FTA Grant: N/A

Construction

Description: Install one track crossover (723); Relocate catenary and signal power at several locations, perform modifications and removal of poles; Third-rail installation; Signal infrastructure installation, cable pulls, tie-ins, testing and cut-over of the 723 crossover incorporating changes to “F” Interlocking and software and hardware modification for the Penn Station Central Control (PSCC); Clear debris, materials and other incidentals from the Amtrak Maintenance-of-Way Yard; electrical traction relocations in support of the CH053 civil work.



Transfer Catenary Wire B-923 in Sunnyside Yard.

Schedule and Cost

Progress:

1. Completed installation of sectionalizing switch at Sunnyside Yard Track 2 and placed in service
2. Completed installation of grounding wire for the erected catenary structure located East of 43rd St. Bridge
3. Completed catenary wire relocation at the B-923W structure in Sunnyside Yard
4. Completed erecting five Catenary structures at Tri-vent area(subset “F”)
5. Began installation of conduits for relocating AC negative return at Tri-vent area
6. Installed sectionalizing switch at Loop A near 43rd St. bridge

Project Initiative (P.I.) status:

- P.I. for Communication and Signal, O1-O2 Switch and #51 switch – Completed
- P.I. for 68 Spur track - Completed
- P.I. for Stage 1 ET Catenary – In Progress

Schedule: The schedule was revised based upon the CH053 rebaseline schedule. The catenary work drives the substantial completion date.

Cost: The budget increased due to the scope transfer of the signal trough installation from Harold Stage 2 to Harold Stage 1 plus the addition of the 68 spur track and 51 Switch.

Critical Issues: Completion of final design for electric traction and subsequent Amtrak approval. CH053 work has not progressed as planned because of electrical traction design issues. This issue directly impacts the FHA01 substantial completion date.

Milestones FHA01

Milestones Description	Current Approved Plan*	Actual/Forecast
100% Design	11/19/07	11/19/07 (A)
Commence Stage 1	06/30/07	06/30/07 (A)
Cutover Switch O1/O2	08/10/08	10/05/08 (A)
Compl. 68 Spur Track	05/01/10	05/15/10
Compl. Signal Power Cable	07/20/10	09/05/10
Substantial Completion	01/03/12	01/03/12

Schedule Progress	1st Qtr		Cumulative	
Cost Expenditure	Plan**	Actual	Plan**	Actual
Percent Complete	2.0/3.8	2.2	51.8/55.6	51.8

* Milestone dates are changed to rebaseline schedule

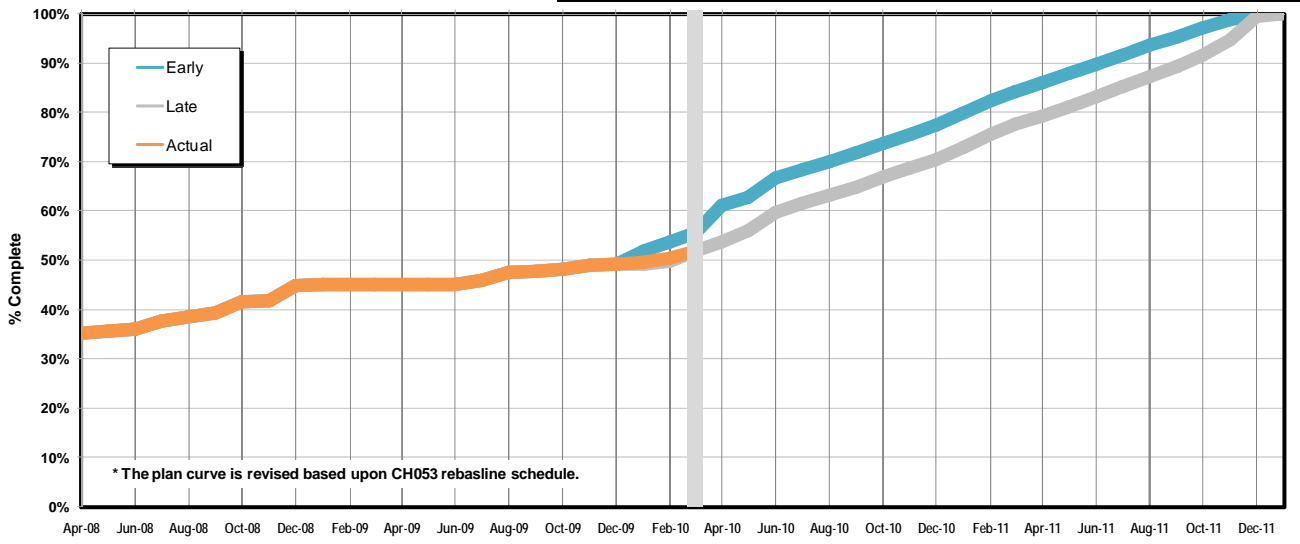
**Late/ Early Plan

Contract Costs (\$000) as of 03/31/10

	Stage 1	F/A Support	Total
Budget at Award	12,100	N/A	12,100
Current Budget	16,825	N/A	16,825
Original Award	12,448	N/A	12,448
Approved Changes	1,500	N/A	1,500
Current Contract Value	13,948	N/A	13,948
Invoiced Amount	10,924	N/A	10,924
Estimate at Completion	16,825	N/A	16,825

Quarterly Review Report – 1st Quarter '10

Progress Curve (FHA01)



Coordination Required

Real Estate: None required.

Permits: None required.

Railroad Agencies: LIRR and Amtrak.

MOU: Amtrak Force Account Agreement has been signed.

Site Map



FHA02 – Harold Early Stage 2 – Amtrak Direct F / A

Jan, Feb, Mar '10

Contractor: Amtrak

Construction Manager: Peter Malvese

FTA Grant: N/A

Construction

Description: Convert air switches 735, 737, 741, and 743 from air to electric; Install new electric switch machines; Install temporary signal case, cable pulls, tie-ins, testing and cut-over switches; Install signal troughs, pull boxes, 9-ways/conduits, and case foundations for F1 and F2 Central Instrument Houses.



Material Handling of Signal Trough East of Thomson Ave.



Installation of Retaining Wall East of Thomson Ave.

Schedule and Cost

Progress:

1. Completed installation of ground grids for F2D signal case
2. Continued installation of pull boxes, troughs, conduits and 9-way ducts for F1/F2 Central Instrument House
3. Continued to revise Stage 2 estimate and schedule based on the 100% design

Project Initiative (P.I.) Status:

P.I. for Early Stage 2 Signal Work – Completed
 P.I. for the remaining Stage 2 Work – In progress

Schedule: Progress ahead of schedule since the dedicated C&S gang increased. Schedule will be revised once the P.I. for the remaining Stage 2 work is approved.

Cost: The Early Stage 2 figures represent the current value of the PI for the early work. These figures will be finalized once the P.I. for remaining Stage 2 work is approved.

Critical Issues: None

Milestones Early FHA02

Milestones Description	Current Approved Plan	Actual/Forecast
Commence Early Stage 2	12/15/08	12/15/08 (A)
Cutover Switch Machine-741	06/13/09	06/06/09 (A)
Cutover Switch Machine-743	06/21/09	06/20/09 (A)
Cutover Switch Machine-735	07/11/09	05/30/09 (A)
Cutover Switch Machine-737	07/18/09	06/14/09 (A)
Complete Early Stage 2	08/25/10	05/05/10

Schedule Progress	1st Qtr		Cumulative	
Cost Expenditure	Plan*	Actual	Plan*	Actual
Percent Complete	15.3/19.0	11.1	51.7/73.5	96.1

*Late/ Early Plan

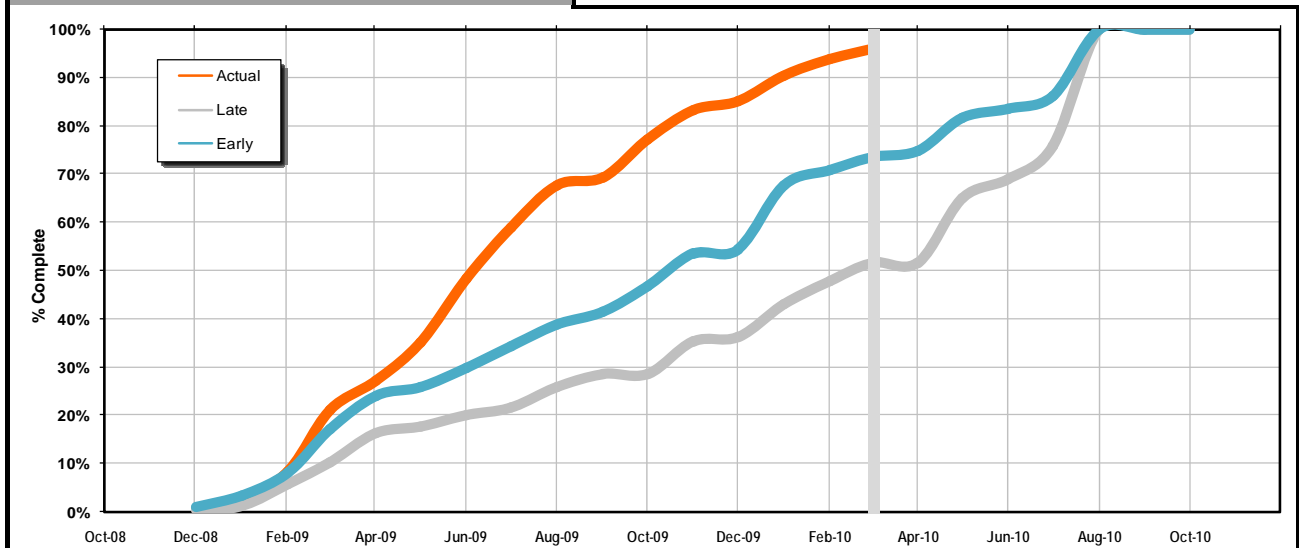
Contract Costs (\$000) as of 03/31/10

	Early Stage 2	F/A Support	Total
Budget at Award**	4,161	N/A	4,161
Current Budget**	4,161	N/A	4,161
Original Award	4,161	N/A	4,161
Approved Changes	-	N/A	-
Current Contract Value	4,161	N/A	4,161
Invoiced Amount	3,578	N/A	3,578
Estimate at Completion	3,798	N/A	3,798

**Note: Stage 2 partial budget (early work only)

Quarterly Review Report – 1st Quarter '10

Progress Curve (FHA02)



Coordination Required

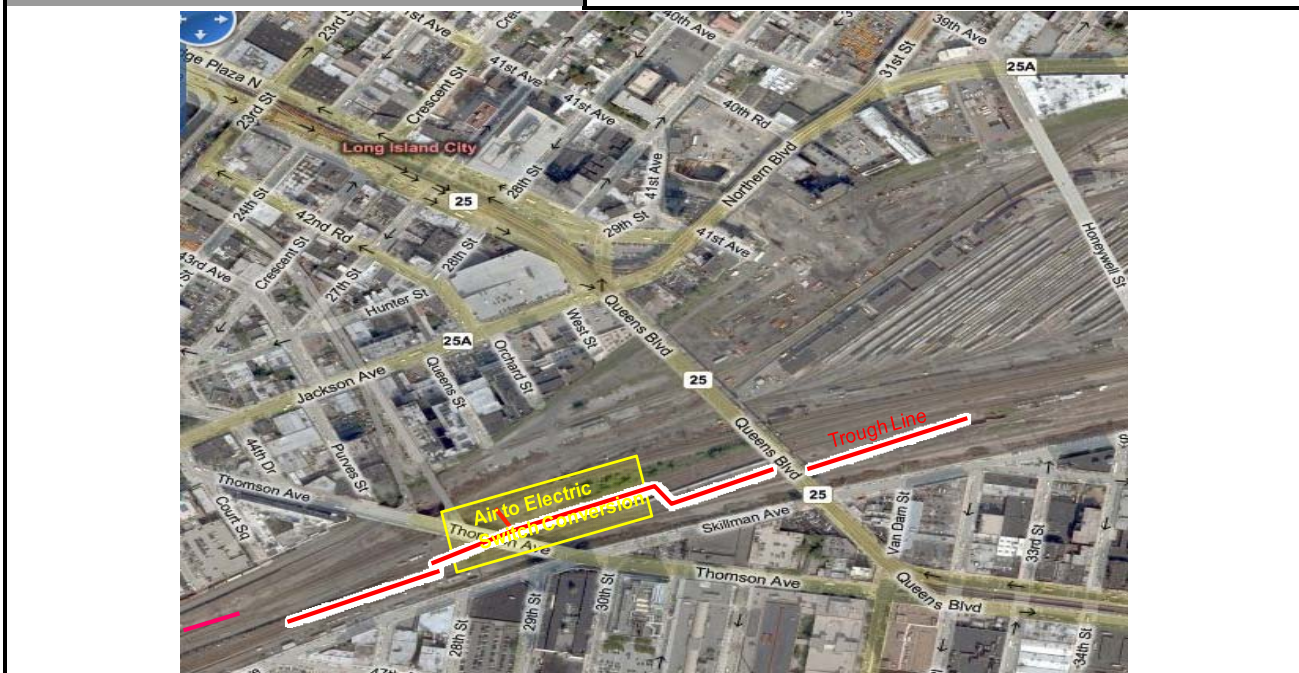
Real Estate: None required.

Permits: None required.

Railroad Agencies: LIRR and Amtrak.

MOU: Amtrak Force Account Agreement has been signed.

Site Map



FHL01 - Harold Stage 1 - LIRR Direct Force Account

Jan, Feb, Mar '10

Contractor: LIRR

Construction Manager: Peter Malvese

FTA Grant: NY-03-0344

Description: Install eight track switches into the existing signal system; remove two switches; Install signal infrastructure in preparation of Stage 2 (Harold and Point CILs); Reroute signal power cables; Install third rail for new switches & track. Pull traction power cables from G02 Substation final manholes to Harold Main Line tracks; Energize and commission the relocated G02 Substation installed by CH053. Install and relocate signal and communications cables. Cut and throw Main Line 4 and relocated Main Line 2 upon CH053 expanding the South ROW East of 43rd Street.

Construction



Pulling Communication Cable at 43rd Street.



Trenching Conduit and Pull Box for AC Negative Return.

Schedule and Cost

Progress:

- Continued circuit revisions at Harold CIL and T2A signal hut for Switch 865
- Continued temporary communication work
- Conducted planning sessions for 2010 track work construction
- Survey and stakeouts for upcoming work at 823 crossover

Schedule: The schedule is revised based upon CH053 rebaseline schedule.

Cost: Approved changes reflect the scope transfer of the M2 switch installation from Stage 1 to Stage 3.

Critical Issues: Completion of final design for electric traction and subsequent Amtrak approval. CH053 work has not progressed as planned because of electrical traction design issue. This work impacts the FHL01 substantial completion date.

Milestones FHL01

Milestones Description	Current Approved Plan*	Actual/Forecast
100% Design	07/07 (A)	07/07 (A)
Begin Stage 1	06/29/07	06/29/07 (A)
Install 823 Switch	03/20/10	08/29/10
Compl. Signal Power Cable	09/20/10	11/22/10
Cutover 821 & 823 Switch	03/05/11	03/5/11
Cutover 865 Switch	10/16/10	10/16/10
Substantial Completion	10/10/11	10/10/11

Schedule Progress	1st Qtr		Cumulative	
Cost Expenditure	Plan**	Actual	Plan**	Actual
Percent Complete	1.1/9.1	1.1	52.0/60.0	51.9

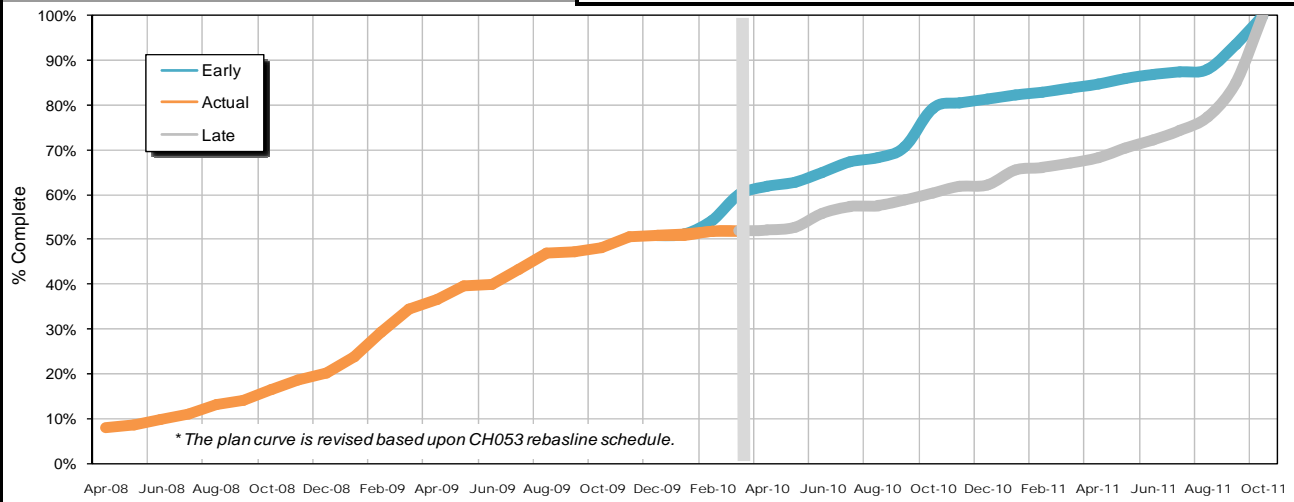
* Milestone dates are changed to rebaseline schedule

**Late/Early Plan

Contract Costs (\$000) as of 03/31/10

	Stage 1	F/A Support	Total
Budget at Award	28,781	N/A	28,781
Current Budget	21,972	N/A	21,972
Original Award	28,781	N/A	28,781
Approved Changes	(7,999)	N/A	(7,999)
Current Contract Value	20,782	N/A	20,782
Invoiced Amount	9,234	N/A	9,234
Estimate at Completion	21,972	N/A	21,972

Progress Curve (FHL01)



Coordination Issues

Real Estate: None required.

Railroad Agencies: LIRR and Amtrak.

MOU: Amtrak Force Account Agreement signed. LIRR Force Account Agreement signed.

Permits: None required.

Site Map



FHL02 - Harold Early Stage 2 LIRR Direct F/A

Jan, Feb, Mar '10

Contractor: LIRR

Construction Manager: Peter Malvese

FTA Grant: NY-03-0344

Construction

Description: Advanced signal work for Stage 2 including cable trough, conduit and pull box installations, located in Point Interlocking and the West End of Harold Interlocking. Scope includes signal training for 4th signal gang.



New Signal Foundation Installation at Hunterspoint Ave.



Trough Installation at East of Queens Blvd.

Schedule and Cost

Progress:

1. Completed installation of pedestal foundations for 822EA and 828W signals
2. Continued installation of trough, pull boxes and conduits at East of Thomson Ave.
3. Continued developing the Stage 2 estimate and schedule based on 100% Signal design

Schedule: Progress is on schedule. The schedule will be revised once the Memorandum of Understanding (MOU) from LIRR for the balance of work is approved.

Cost: The Early Stage 2 numbers represent the current value of the MOU for the early work. These cost numbers will be revised once the MOU for remaining Stage 2 is approved.

Critical Issues: None

Milestones FHL02

Milestones Description	Current Approved Plan	Actual/ Forecast
Commence Stage 2	08/17/09	08/17/09 (A)
Compl. Trough - Point CIL	03/03/10	05/07/10
Compl. Trough - H4 CIL	12/08/10	04/28/10
Complete. Early Stage 2	04/09/10	05/21/10

Schedule Progress	1st Qtr		Cumulative	
Cost Expenditure	Plan*	Actual	Plan*	Actual
Percent Complete	18.0/21.0	28.0	67.0/86.6	72.6

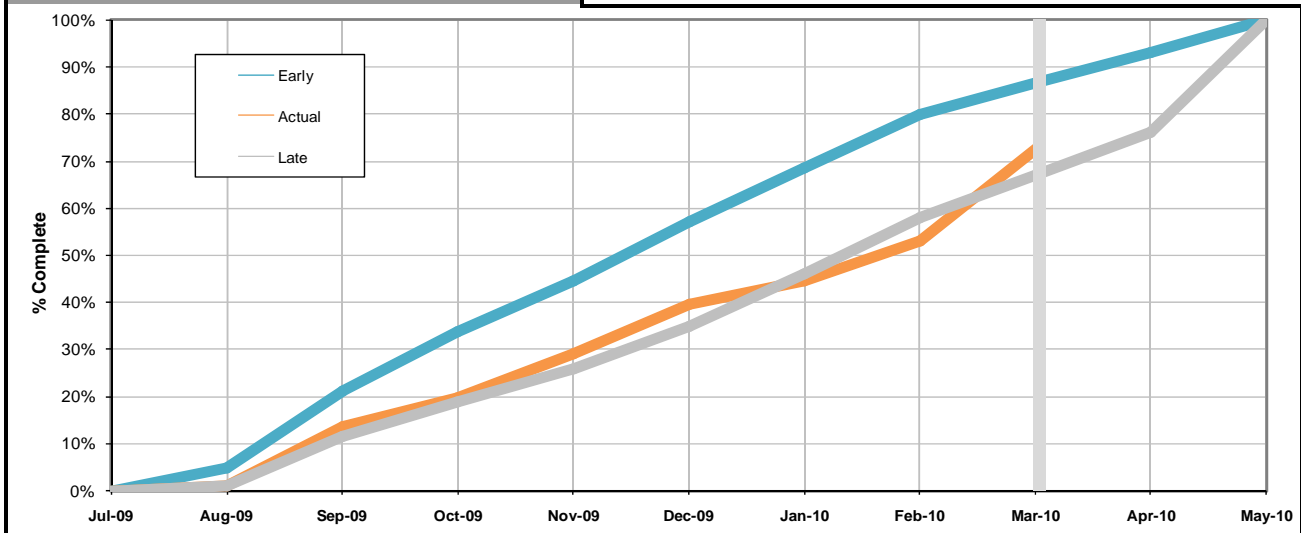
*Late/ Early Plan

Contract Costs (\$000) as of 03/31/10

	Early Stage 2*	Signal Training	Total
Budget at Award	1,634	3,429	5,063
Current Budget	1,634	3,429	5,063
Original Award	1,634	3,429	5,063
Approved Changes			
Current Contract Value	1,634	3,429	5,063
Invoiced Amount	851	2,637	3,488
Estimate at Completion	1,340	3,429	4,769

*Note: Stage 2 partial budget (early work only)

Progress Curve (FHL02)



Coordination Issues

Real Estate: None required.

Railroad Agencies: LIRR and Amtrak.

MOU: Amtrak Force Account Agreement signed. LIRR Force Account Agreement signed.

Permits: None required.

Site Map



FHA62 – F Interlocking CIH

Jan, Feb, Mar '10

Contractor: Amtrak

Construction Manager: Chuck Miller

FTA Grant: N/A

Construction

Description: Amtrak manufacture and delivery of Central Instrumentation Houses (CIHs) and cases for the F2 and F1 Interlockings. All work will be performed by Amtrak.



Signal Cases and F2 CIH in Amtrak Signal Facility in Lancaster, PA.

Schedule and Cost

Progress:

1. F1F hut was completed and is being stored at Amtrak's Lancaster facility.
2. Work on the local control panel is being progressed, to be incorporated into the F CIH.

Schedule:

Due to other Amtrak work priorities, delivery of the F2 and F1 CIHs are now scheduled for Oct. '10 and Jan '11, respectively. The reallocation of Amtrak's labor resources to this work has impacted the schedule. The effects of economic stimulus money (project completion required ahead of ESA) and the limited Amtrak resources to perform the work may further impact the CIH deliveries. Amtrak is reassessing their work to accommodate ESA and will advise whether overtime will support the current schedule. Assessment is targeted for completion by April 9th. Currently, no impact to the overall schedule is anticipated.

Cost: Project is within budget.

Critical Issues: Obtain labor resource commitment from Amtrak to maintain schedule.

Milestones FHA62

Milestones Description	Current Approved Plan	Actual/ Forecast
100% Design	08/28/08	08/28/08 (A)
NTP	09/26/08	09/26/08 (A)
Delivery of F2 CIL	12/31/09	10/31/10
Delivery of F1 CIL	03/31/10	01/31/11
Substantial Completion	08/31/10	02/11/11
Close-Out	10/31/10	01/31/12

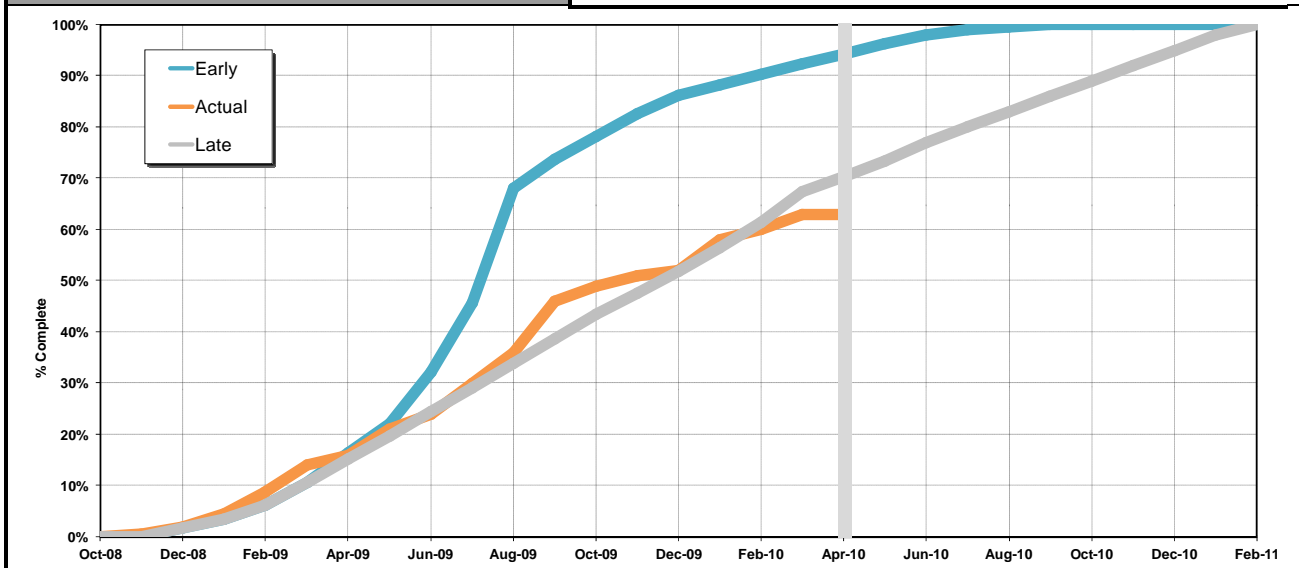
Schedule Progress	1st Qtr		Cumulative	
Cost Expenditure	Plan*	Actual	Plan*	Actual
Percent Complete	6.0/14.0	5.0	70.4/94.3	63.0

* Late/ Early Plan

Contract Costs (\$000) as of 03/31/10

	FHA62	F/A Support	Total
Budget at Award	8,178	N/A	8,178
Current Budget	8,178	N/A	8,178
Original Award	6,954	N/A	6,954
Approved Changes			
Current Contract Value	6,954	N/A	6,954
Invoiced Amount	3,983	N/A	3,983
Estimate at Completion	8,178	N/A	8,178

Progress Curve (FHA62)



Coordination Issues

Real Estate: N/A

Railroad Agencies: Amtrak

MOU: Amtrak Force Account Agreement signed. Amtrak Project Initiative required signed.

Permits: N/A

Site Map



Quarterly Review Report – 1st Quarter '10

VHA/L02 – Procure Harold Materials – Stage 2

Jan, Feb, Mar '10

Procurement

Contractor: Various

FTA Grant: ARRA Stimulus Funds NY-36-0002 (Only VHL02)

Construction Manager: Mark Denise

Description: Procure LIRR and Amtrak Force Account material in support of Stage 2 construction. LIRR will manage and operate the project warehouse with ESA oversight. Example of materials include: *Track* - switches, rail, concrete ties and ballast; *Third-Rail & Power* – rail, cables, boards, brackets, wire, power connectors, bus bar; *Signal Equipment* – switch machines, cables, signals, relays, heaters, troughs, impedance bonds; and *Structures* – signal bridges, retaining walls etc.; *Communications* – equipment, wire, cables, and poles.



Force Account Materials at Warehouse Storage Area.

Schedule and Cost

Status: Cable and wire bids for VHA02 are open and are in the award process. Material deliveries are expected between July and August. VHA/L02 material procurements for signal, track, third rail, ET/Catenary continue and will increase in frequency during the second quarter of '10.

Cost: Table shows value of awards to date. An overall VHL02 savings is expected due to large scale advance procurement for Stage 2 materials.

Critical Issues: Working with the GEC to finalize material requirements to ensure that material is available to meet our schedule.

Amtrak – VHA02 – Budget Summary (03/31/10)

Material	Budget	Harold Stage 2 Awarded PO Value to Date	Percent Procurement Completed	Estimate at Completion
Signals	6,074,955	846,422	14%	6,074,955
Track	3,434,215	1,559	.02%	3,434,215
Third Rail	491,632	7,170	2%	491,632
DC Traction	431,615	0	0%	431,615
Communication	126,716	0	0%	126,716
Catenary	553,007	1,740	1%	553,100
Elec. Traction	2,106,948	283,200	13%	2,106,948
Totals	13,219,087	1,140,091	9%	13,219,181

LIRR – VHL02 – Budget Summary (03/31/10)

Material	Budget	Harold Stage 2 Awarded PO Value to Date	Percent Procurement Completed	Estimate at Completion
*Signals	10,444,523	4,390,426	74%	5,924,985
*Track	13,264,679	198,635	1%	13,290,357
*Third Rail	1,944,087	912,437	100%	912,087
Communication	510,111	0	0%	510,111
Totals	26,163,400	5,501,498	27%	20,637,540

CS099 – Advanced Procurement of Third Party Materials

Jan, Feb, Mar '10

Contractor: Various

Construction Manager: Mark Denise

FTA Grant: ARRA Stimulus Funds NY-36-0002

Procurement

Description: Advanced procurement of Third Party material in support of future tunneling contracts. LIRR will manage and operate the project warehouse with ESA oversight. Examples of materials include: *Track – 136 pound continuously welded rail (CWR); Third-Rail & Power – composite rail, cable, protection boards, brackets, power connectors; Signal Equipment – cables, signals, relays, heaters, impedance bonds.*



Materials at Warehouse Storage Area.

Schedule and Cost

Status: Third party materials are being received at the new project warehouse. These materials are being segregated from force account materials for easy identification. All material will be on site within the next 4 weeks.

Cost: Table shows awards to date. The intent of our advanced procurement of materials has been met successfully with significant savings, both in direct material costs and cost avoidances.

Critical Issues: Receipts and payments are being processed in accordance with the grant provisions

CS099 – Budget Summary (03/31/10)				
Material	Budget	Stimulus Awarded PO Value to Date	Percent Procurement Completed	Estimate at Completion
CS099	37,874,133	17,178,897	55%	31,093,772

VH051 (Part 1) - Harold and Point CILs

Jan, Feb, Mar '10

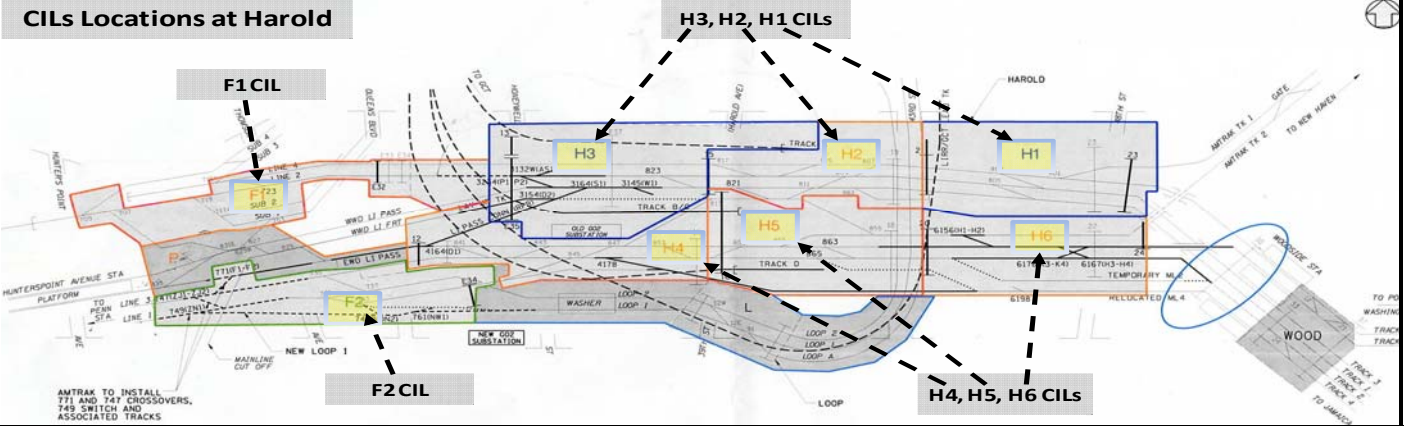
Contractor: Ansaldo STS USA, Inc
Construction Manager: Chuck Miller

FTA Grant: NY-03-0344

Construction

Description: This is the procurement of seven Harold Interlocking Central Instrument Locations (CILs) and one Point Interlocking Central Instrument Location and the purchase of a fully operational Vital Microprocessor-Based Interlocking Control System for the referenced interlockings. This system will operate the new Harold Interlocking configuration and eventually connect to the new GCT tunnels.

CILs Locations at Harold



Schedule and Cost

Progress:

- Contractor is continuing to forward the general contract submittals along with the specific POINT interlocking submittals; Book of Plans, System Segment Design Document, Preliminary designs for: Vital Network, System Architecture, Electromagnetic Compatibility, Appliance Interface, Safety Assurance Program Plan, Final Cable Plan and Cable Routing Verification and Tie-in Circuits.
- Miscellaneous products continue to be submitted by the vendor for railroad review and approval.
- Continuing dialogue on technical items, revision of terminal board layout and Faraday cages, use of Communication and Synchronization Printed Circuit Boards, location of Sunnyside Yard signal, effort for Factory and On-site acceptance testing.
- Continuing review of the schedule, location/site information and systems in place in order to install and cut-over POINT Interlocking.

Schedule:

- The updated schedule has been approved (reflecting the impact of the revised control lines) and a modification to the contracts Schedule Milestones is being drafted. The modification will be forwarded to ASTS by the end of April with a request for proposal for ASTS to acknowledge any impacts to their work. Milestones noted above are per the modification.
- A joint meeting of ARINC, ASTS, the LIRR and ESA will take place on April 20th for a detailed schedule discussion.

Cost: See contract costs.

Critical Issues: None.

Milestones VH051 (Part 1)

Milestones Description	Current Approved Plan	Actual/Forecast
NTP	05/11/09	05/11/09 (A)
Compl. Point Huts & Cases	07/27/10	09/17/10
Compl. H4-H6 Huts & Cases	12/24/10	08/18/11
Compl. Point CIL	06/30/10	04/20/11
Compl. H1-H3 Huts & Cases	01/25/11	11/07/11
Compl. H4 CIL	04/26/11	10/20/11
Compl. H5 CIL	05/26/11	01/05/12
Compl. H6 CIL, 30 Loc Hut	07/26/11	02/02/12
Compl. H1, H2, H3 CILs	10/26/11	05/03/12
Substantial Completion	06/25/12	09/24/13
Final Completion	07/25/12	10/24/13

Schedule Progress	1st Qtr		Cumulative	
Cost Expenditure	Plan*	Actual	Plan*	Actual
Percent Complete	1.6/3.0	2.8	3.8/10.0	7.0

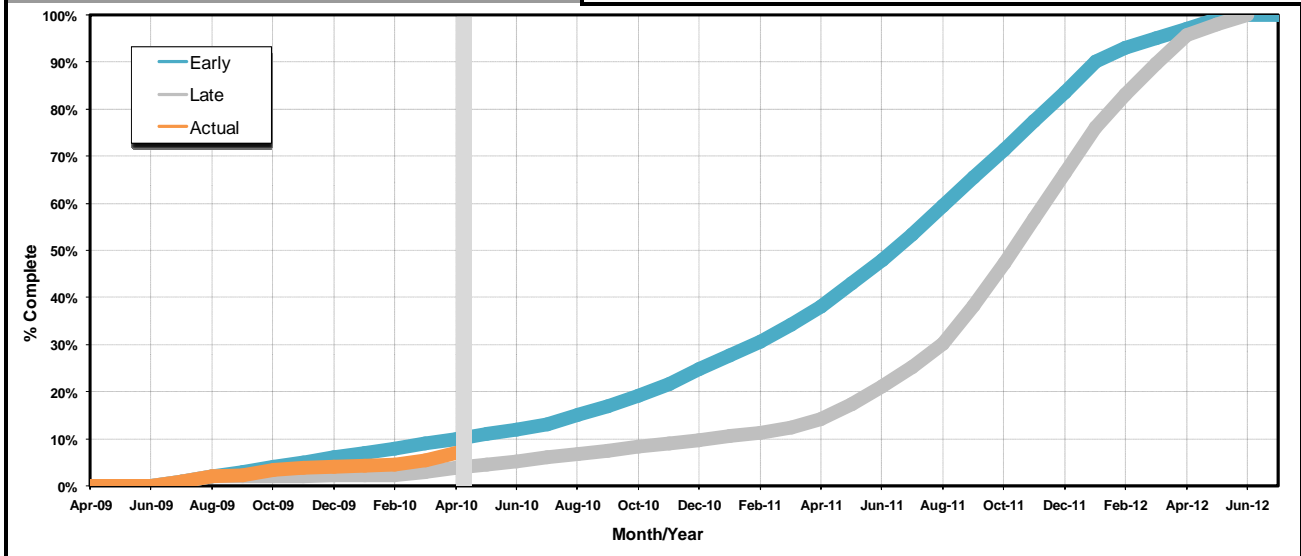
* Late/ Early Plan

Contract Costs (\$000) as of 03/31/10

	VH051-1	F/A Support	Total
Budget at Award	30,891	N/A	30,891
Current Budget	30,753	N/A	30,753
Original Award	25,840	N/A	25,840
Approved Changes		N/A	
Current Contract Value	25,840	N/A	25,840
Invoiced Amount	1,109	N/A	1,109
Estimate at Completion	30,753	N/A	30,753

Quarterly Review Report – 1st Quarter '10

Progress Curve (VH051 Part 1)



Coordination Issues

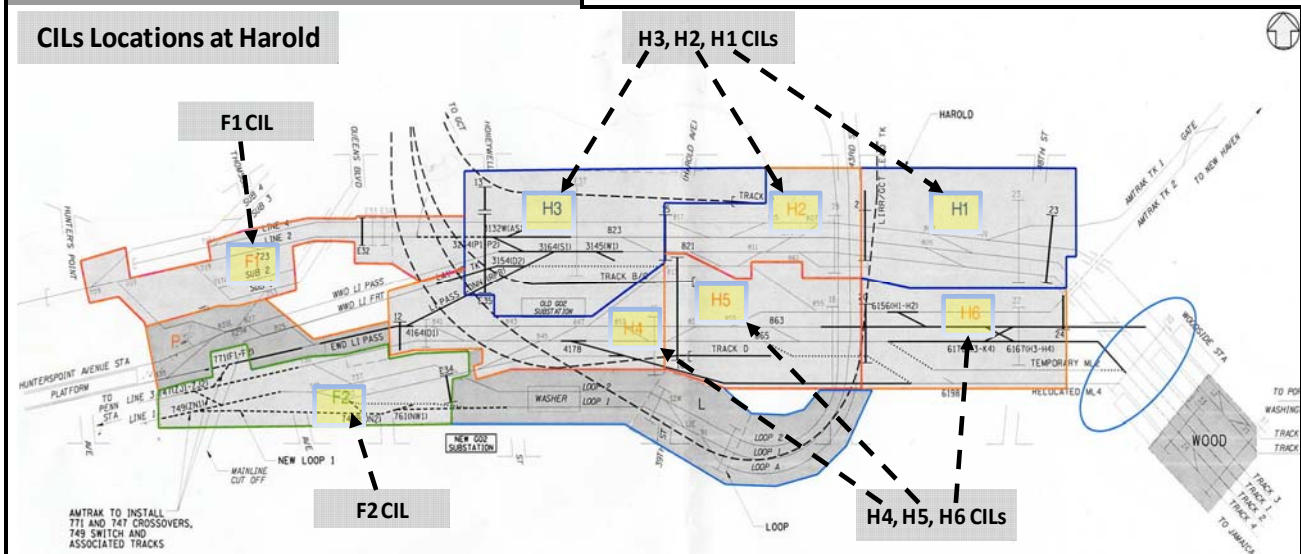
Real Estate: N/A

Permits: N/A

Railroad Agencies: LIRR/ Amtrak

MOU: LIRR Force Account MOU signed.

Site Map



VH051 (Part 2) – Harold Tower Supervisory Control System

Jan, Feb, Mar '10

Contractor: ARINC, Inc.

Construction Manager: Chuck Miller

FTA Grant: NY-03-0344

Construction

Description: This procurement contract requires purchase of a fully operational Harold Tower Supervisory Control System (HTSCS) for control of signals at Harold and Point Interlockings.



Supervisory Control System (Sample).



Harold Tower Supervisory Control System - Existing Tower.

Schedule and Cost

Progress:

1. The F Harold Alternate Control System change has been submitted to ARINC with a request for proposal. ARINC has advised that the proposal will take approximately 6 weeks to prepare and return. Receipt is scheduled for the second week in May.
2. Continued to work with the Central Instrument Location (CIL) contractor on the interface design detailing system responsibility (vendors are exploring options to take responsibility for work originally in the other's contract, if it is reasonable and more efficient to do so) and verifying system operation, installation and testing of the local control systems.
3. Factory Acceptance Testing of the Harold Tower Control System Segment (HTSCS) is planned for May '10

Schedule:

1. Receipt of the proposal for the F' Harold Alternate Control System (FHACS) will require revision of ARINC's delivery schedule. ARINC is reviewing the FHACS scope of work and, as noted above, is targeting the second week of May for completion of their review and submittal of their estimate.
2. Final completion of the ARINC work will track with the requirements and revised in-service dates for the CILs.
3. A joint meeting of ARINC, ASTS, the LIRR and ESA will take place on April 20th for a detailed schedule discussion.

Cost: Progressing on budget. (There is no current additional cost impact for the FHACS change.)

Critical Issues: None.

Milestones VH051 (Part 2)

Milestones Description	Current Approved Plan	Actual/Forecast
Advertise	03/27/08	03/27/08 (A)
NTP	02/03/09	02/03/09 (A)
Delivery of Control Systems	03/16/10	05/30/10
Substantial Completion	08/24/10	12/09/10
Final Completion	05/03/12	05/03/12

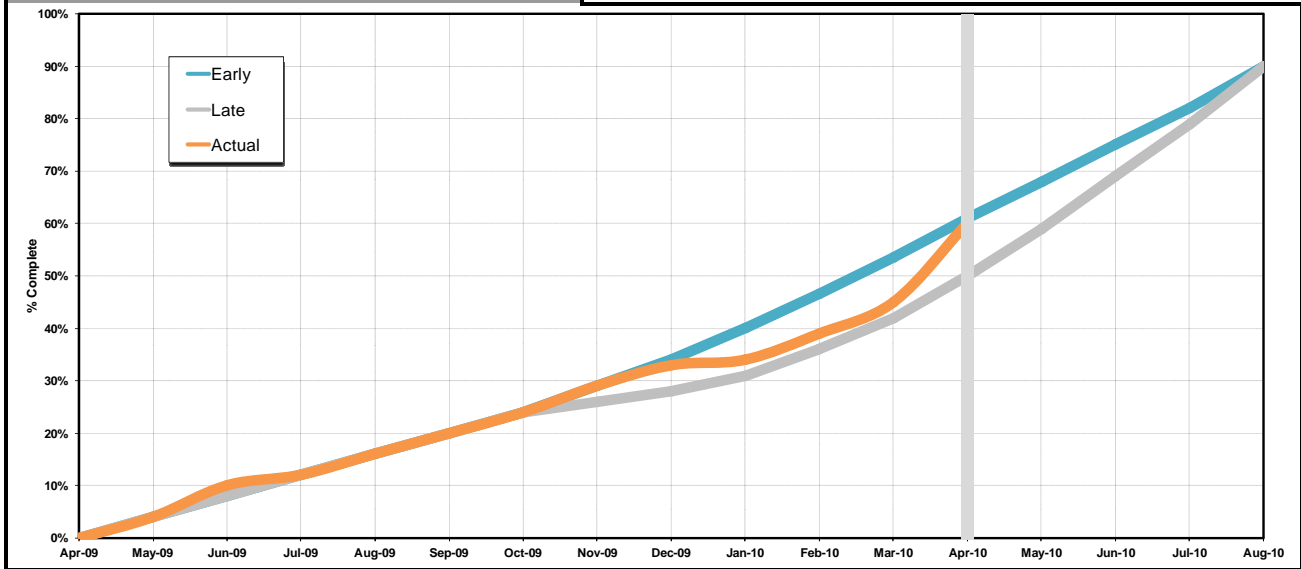
Schedule Progress	1st Qtr		Cumulative	
	Plan	Actual	Plan	Actual
Cost Expenditure				
Percent Complete	19.0/21.0	26.0	50.0/61.0	60.0

* Late/ Early Plan

Contract Costs (\$000) as of 03/31/10

	VH051-2	F/A Support	Total
Budget at Award	7,061	N/A	7,061
Current Budget	7,061	N/A	7,061
Original Award	5,354	N/A	5,354
Approved Changes		N/A	
Current Contract Value	5,354	N/A	5,354
Invoiced Amount	1,603	N/A	1,603
Estimate at Completion	7,061	N/A	7,061

Progress Curve (VH051 Part 2)



Coordination Issues

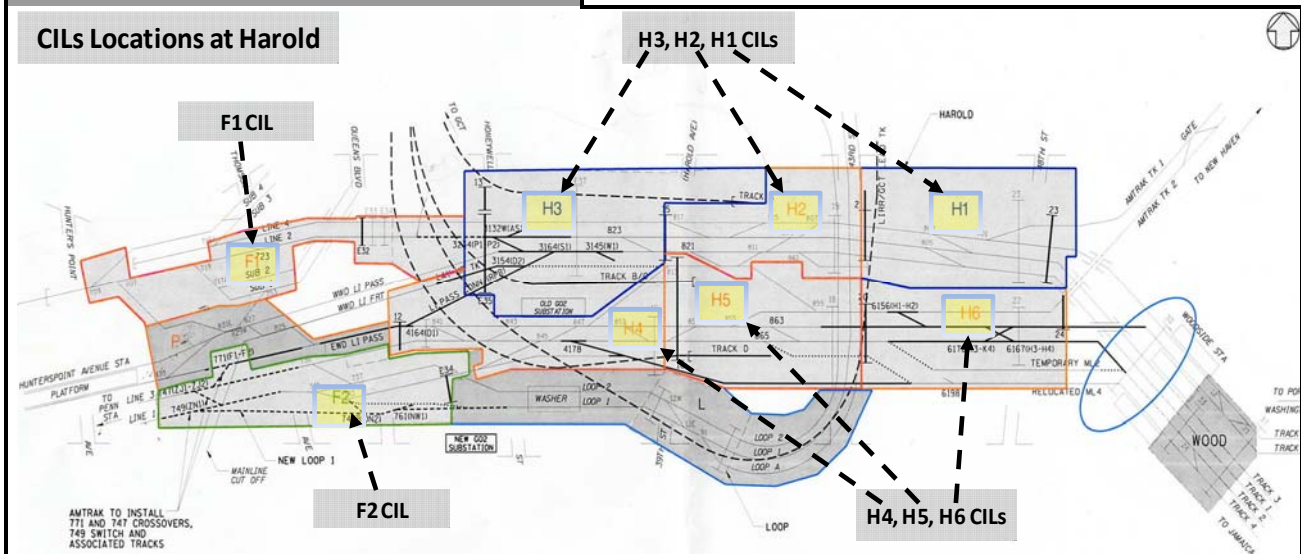
Real Estate: N/A

Permits: N/A

Railroad Agencies: LIRR.

MOU: LIRR Force Account MOU signed.

Site Map



Projects – To Be Awarded

VM014 – Vertical Circulation (Escalators & Elevators)

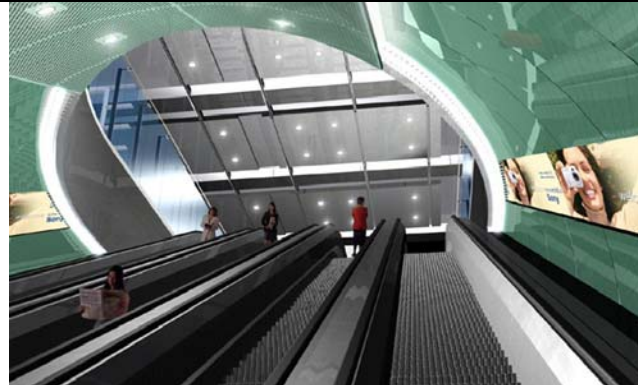
Jan, Feb, Mar '10

Contractor: TBD

Construction Manager: TBD

FTA Grant: NY-03-0344

Description: Install 21 elevators, 45 escalators and compactor lift in Concourse, Caverns, 44th & 50th Street Ventilation Facilities, MNR facilities and entrance.



To Be Awarded

Schedule and Cost

Progress:

1. Request for Proposals (RFP) issued Jan '10
2. Proposals received on March 26th
3. Evaluation of Proposals begun on March 29th

Schedule: See forecast of project milestones.

Cost: Budget to be included upon award.

Critical Issues: None.

Milestones VM014		
Milestones Description	Current Approved Plan	Actual/Forecast
Advertise (IFB)	12/09	1/10 (A)
NTP	07/10	07/10 (F)
Substantial Completion		
Close-Out		

Schedule Progress	1st Qtr		Cumulative	
Cost Expenditure	Plan	Actual	Plan	Actual
Percent Complete				

Contract Costs (\$000) as of 03/31/10			
	VM014	F/A Support	Total
Budget at Award			
Current Budget	TBD		TBD
Original Award			
Approved Changes			
Current Contract Value			
Invoiced Amount			
Estimate at Completion	TBD		TBD

Coordination Required

Real Estate:

Railroad Agencies: MNR and LIRR

Permits: N/A

MOU: N/A

Summary of Completed Contracts

NTP	Substantial Completion			% Complete		Original Budget at Award (\$M)	Final (\$M)
	Plan	Revised	Forecast	Plan	Actual		
QUEENS							
<i>CQ025 – Demolition of Superior Reed Bldg and Preparation of Yard A</i>						<i>Tully Environmental, Inc.</i>	
11/5/02	7/2/03	9/15/03	9/15/03 (A)	100.0%	100.0%	5.3	5.6
<i>CQ026 – Open Cut Excavation at Bellmouth</i>						<i>Kiewit Construction, Inc.</i>	
10/7/02	9/7/03	11/25/03	11/25/03 (A)	100.0%	100.0%	16.8	18.7
<i>CQ027 – Arch St Yard and Shop Facility*</i>						<i>Slattery Skanska/Edwards & Kelcey</i>	
7/9/02	6/28/04	11/30/04	12/31/04 (A)	100.0%	100.0%	77.9	77.1
<i>CQ028 – Queens Open-Cut Excavation</i>						<i>Pile Foundation Construction Co., Inc.</i>	
04/27/06	04/15/08	10/22/08	06/08 (A)	100.0%	100.0%	121.5	62.8
<i>CQE28-01 – Emergency Work Civil/Structural</i>						<i>Railroad Construction Co., Inc.</i>	
06/08	10/09	10/09	12/09	100.0%	100.0%	6.8	9.2
<i>CQE28-02 – Queens Emergency Work – Environmental</i>						<i>Impact Environmental Consulting, Inc.</i>	
07/14/08	08/09	01/10	02/10	100.0%	100.0%	8.4	2.1
<i>FQA36 – Arch Street Yard Access – Amtrak</i>						<i>Amtrak F/A</i>	
11/14/04	09/01/05	06/23/06	06/23/06 (A)	100.0%	100.0%	3.8	3.4
<i>FQL36 – Arch Street Yard Access – LIRR</i>						<i>LIRR F/A</i>	
11/14/04	09/01/05	06/23/06	06/23/06 (A)	100.0%	100.0%	8.3	4.9
<i>FQL35 – Wood Interlocking – LIRR*</i>						<i>LIRR F/A</i>	
07/05	10/08	10/08	11/08 (A)	100.0%	100.0%	26.9	27.2
<i>VH055 – Switch Exchange System (Procurement)</i>						<i>Plasser American Corp.</i>	
12/20/02	06/27/04	05/29/05	09/30/05 (A)	100.0%	100.0%	17.7	17.2
<i>VHA01 – Procure Harold Materials – Stage 1 Amtrak</i>						<i>Various</i>	
06/06/06	07/08	29/09/10	09/09 (A)	100.0%	100.0%	5.1	5.2
<i>VHL01 – Procure Harold Materials – Stage 1 LIRR</i>						<i>Various</i>	
6/26/07	01/09	03/10	09/09 (A)	100.0%	100.0%	8.3	6.8
MANHATTAN / BRONX							
<i>CM001 – Highbridge Yard*</i>						<i>Highbridge Yard Contractors</i>	
9/5/01	4/15/03	4/15/03	12/4/03 (A)	100.0%	100.0%	80.1	75.6
<i>CM016 – Manhattan Approach Tunnels Excavation</i>						<i>Kiewit Constructors, Inc.</i>	
2/23/04	8/20/04	8/20/04	8/18/04 (A)	100.0%	100.0%	11.8	11.1
<i>CM017 – GCT East Yard Remediation</i>						<i>Tully Environmental Inc.</i>	
1/6/03	10/3/03	10/3/03	9/26/03 (A)	100.0%	100.0%	2.2	1.8
<i>FMM02 – GCT East Yard Track & Signal Modifications</i>						<i>MNR F/A</i>	
12/01/02	12/09	12/09	12/08 (A)	100.0%	100.0%	55.1	26.8
<i>CS770 – GCT Flat Cars Procurement</i>						<i>J-Track, LLC</i>	
11/17/08	02/09	03/09	03/09 (A)	100.0%	100.0%	2.3	2.3
<i>CS780 – Madison Yard Preparation</i>						<i>T. Moriarty and Sons, Inc.</i>	
05/14/08	12/08	04/09	09/09 (A)	100.0%	100.0%	4.4	4.7
<i>CS800 – GCT Instrumentation</i>						<i>Wang Technology, LLC.</i>	
06/28/08	06/09	09/09	09/09 (A)	100.0%	100.0%	7.1	7.2
<i>VM022 – MNR Locomotives</i>						<i>Brookville Equipment Corp.</i>	
12/1/02	01/09	01/09	06/09 (A)	100.0%	100.0%	5.8	5.5
Total						475.6	375.2

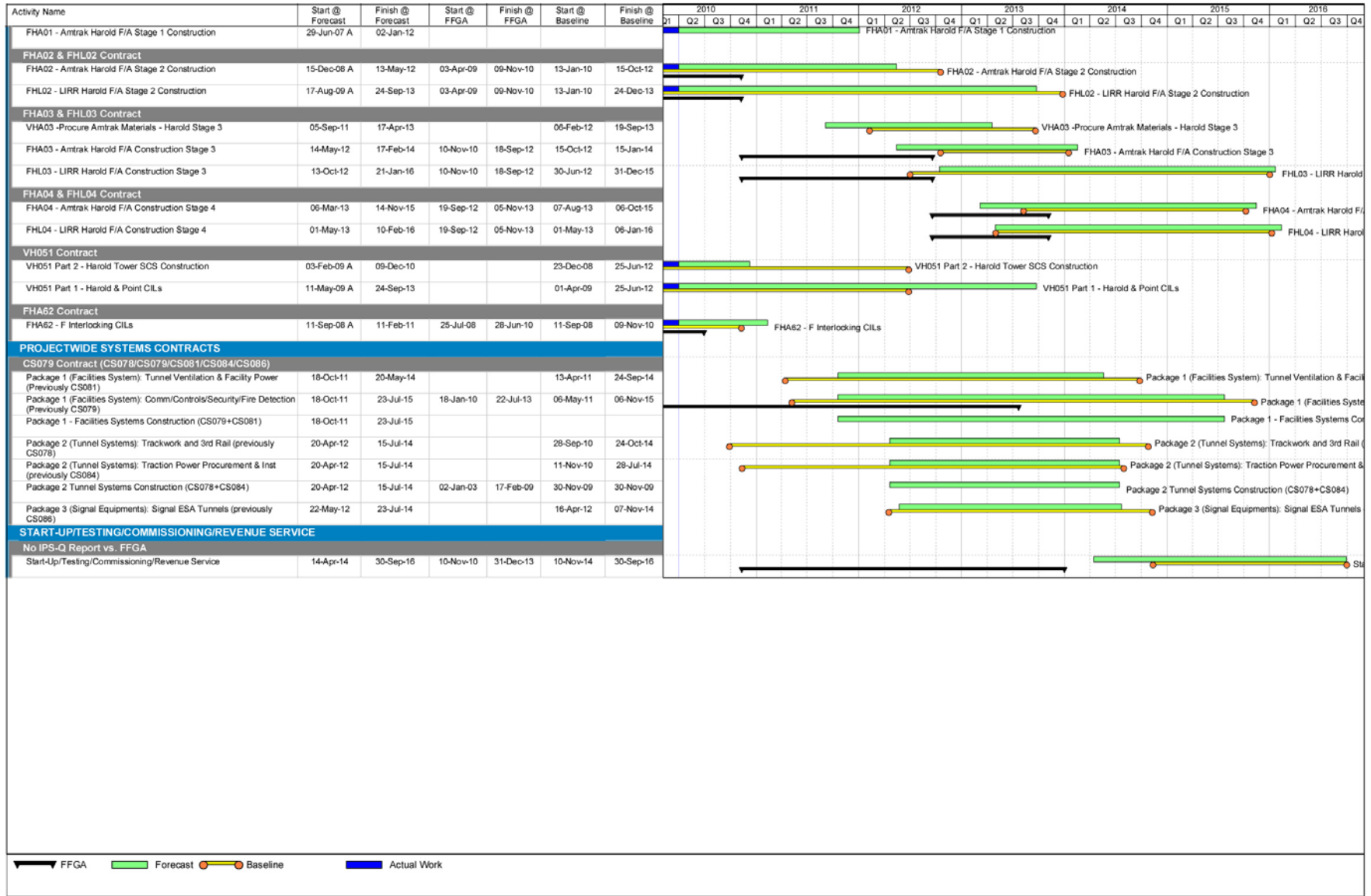
* A review of historical documentation for these contracts resulted in a revision to the original budget at award value.

Quarterly Review Report – 1st Quarter ‘10

Summary Program Schedule



Quarterly Review Report – 1st Quarter ‘10



Quarterly Review Report – 1st Quarter '10

Contract Packaging Plan – Rev. 8.0

Active Pre-2008	2008	2009	2010	2011	2012	2013	2014	2015	2016
<p>CM009 ▲ Manhattan Tunnels Excavation</p>	<p>CM019 ▲ Manhattan Structures 1</p> <p>General Conditions CS790 – GCT Protection ▲</p> <p>FM216 ▲ Traction Power MODs & 13.2kV Loop</p>	<p>CM008A ▲ Madison Yard Site Clearance (LIRR Concourse)</p> <p>CM004 ▲ 44th St. Demolition, Construct Fan Plant Structure and 245 Park Avenue Entrance</p> <p>CM013 IFB ▲ 50th Street Vent Plant Facility</p> <p>General Conditions ▲ CS810 – Instrumentation - Queens</p> <p>CQ031 ▲ Queens Bored Tunnels and Structures</p>	<p>VM014 RFP Vertical Circulation Elements</p> <p>CQ039 RFP Northern Boulevard Crossing Construction</p>	<p>CM014 RFP GCT Concourse and Facilities Fit-Out</p> <p>CM012 INNOVATIVE RFP Manhattan Structures 2 and Facilities Fit-Out</p> <p>CM013A IFB 55th Street Vent Plant Facility</p> <p>VQ065 IFB Loop Interlocking CIL</p> <p>CQ032 IFB Plaza Substation & Queens Structures</p> <p>CH057 IFB Harold Structures – Part 3 EBR-WBBP</p> <p>CS081 RFP Tunnel Ventilation and Facility Power</p> <p>CS079 RFP Communications, Controls, Security and Fire Detection</p>	<p>CQ033 IFB Mid-Day Storage Yard Facility</p> <p>FHL03 / FHA03 Harold F/A Stage 3</p> <p>CH058 IFB Harold Structures - Part 3 – E/B Bypass, D Approach</p> <p>CS086 RFP Signal ESA Tunnels</p>	<p>FHL04 / FHA04 Harold F/A Stage 4</p>	<p>CH059 IFB Harold Structures - Part 4</p>	<p>ESA Projectwide Start-up, Testing & Commissioning</p>	
<p>FHL01 / FHA01 ▲ Harold F/A Stage 1</p>	<p>FHA62 AMTRAK ▲ F Interlocking CILs</p> <p>CH053 ▲ Harold Structures - Part 1 & GO2 Substation</p> <p>VH051 (Part 2) ▲ Harold Tower Supervisory Control System</p>	<p>CH054A ▲ Harold Structures – Part 2A</p> <p>VH051 (Part 1) ▲ Harold & Point CILs</p>	<p>FHL02 / FHA02 Harold F/A Stage 2</p> <p>CS078 IFB Track, 3rd Rail and Tunnel Bench Walk</p> <p>CS084 RFP Traction Power Procurement and Installation</p>	<p>CH057 IFB Harold Structures – Part 3 EBR-WBBP</p>	<p>FHL03 / FHA03 Harold F/A Stage 3</p>	<p>FHL04 / FHA04 Harold F/A Stage 4</p>	<p>CH059 IFB Harold Structures - Part 4</p>	<p>ESA Projectwide Start-up, Testing & Commissioning</p>	
VHA01/VHL01 - VHA02/VHL02 - VHA03/VHL03 - VHA04/VHL04									
Procure Harold Material Stages 1 through 4									

Legend

Contract No. [Color Box]

Description [Color Box]

Color Code

- MANHATTAN [Color Box]
- QUEENS [Color Box]
- HAROLD [Color Box]
- PROJECTWIDE [Color Box]

▲ Awarded

Funding and Budget

Project Budget Summary (As of Mar 31, 2010) (\$ in thousands)

<i>Component</i>	<i>FFGA Budget</i>	<i>Current Budget</i>	<i>Awarded Value</i>	<i>Paid To Date</i>	<i>EAC</i>
EIS & Engineering	\$486,270	\$564,665	\$501,015	\$468,829	\$564,665
Construction	4,441,776	5,595,491	2,800,947	1,156,603	5,595,491
Contracts Awarded	1,838,859	3,329,744	N/A	N/A	
Contracts To Be Awarded	2,602,917	2,265,747	N/A	N/A	
OCIP*	250,000	190,853	91,636	89,467	190,853
Management/General Conditions(FFGA only)	475,652	608,623	350,576	281,722	608,623
Real Estate	165,000	166,368	95,339	94,172	166,368
Rolling Stock	531,202	202,000	0	0	202,000
Total	\$6,349,900	\$7,328,000	\$3,839,513	\$2,090,793	\$7,328,000
Capital Program Authorization	\$4,106,674	\$4,373,951			
2010-2014 Capital Program	\$2,243,226	\$2,954,049			

* OCIP budget does not include \$84 million paid with surety funds that are not included in the project budget.

Federal Funding from the American Recovery and Reinvestment Act

ACEP	Contract No.	Project	Brief Description	Federal Share
G509-01-45	VS099	ESA - Pre-Order Materials - Systems	Advanced procurement of construction materials to be installed by various systems' contractors. Material includes wire and cable, conduit, rail and crossties. Partial funding.	\$16.0
G509-01-18	VHL02	ESA - Pre-Order Materials - Harold Interlocking	Advanced procurement of construction materials for Harold Interlocking, including wire and cable, conduit, rail and crossties. Partial funding.	\$6.7
G509-01-22	CQ031	ESA - Queens Bored Tunnels	Excavate tunnels and ventilation shafts under Sunnyside yard and the LIRR's Mainline Tracks. Partial funding.	\$132.3
G409-01-43	CM004	ESA - 44th Street Vent Structure & 245 Park Avenue Entrance	47 East 44th Street demolition and construction of fan plant structure and 245 Park Avenue Entrance. Partial funding.	\$14.2
G509-01-28	CM008A	ESA - Madison Yard Demolition and Site Clearance	Demolition and site clearance in Madison Yard, including environmental abatement and utility relocation, in GCT. Partial funding.	\$17.2
G409-01-26	CH054A	ESA - Harold Structures Part 2	Construct various civil infrastructure elements at Harold Interlocking and expand the existing LIRR Amtrak ROW for the future TBM tunnels and Main-line track diversions. Partial funding.	\$9.2
Total				\$195.4

- The Federal Stimulus grant was awarded on March 1, 2010. The grant allows for deferred local match.
- MTACC is working with the FTA to develop a strategy to expedite the expenditure of the Federal Stimulus funds. At the contract level, the amount spent may exceed the Federal share for that contract. A grant budget adjustment will be submitted to the FTA to reflect the actual draw down amount.

Project Funding Status (As of Mar 31, 2010)

(\$ in Millions)

Total Budget (exclusive of financing costs) \$7,328.0					
MTA Capital Programs	Budget	Funding			
		Federal			Local
		New Starts		Other	
		New Starts	ARRA		
1995-1999	\$ 157.7	\$ 45.7	0	0	\$ 112.0
2000-2004	1,533.5	564.1	0	28.8	940.6
2005-2009	2,682.8	1,826.9	195.4	21.7	638.8
2010-2014	2,954.0	0	0	0	2,954.0
Total	\$7,328.0	\$2,436.7	195.4	50.5	\$4,645.4

*Totals may not add due to rounding.

Value of Contract Awards	\$3,839.5	
- Federal Share of Contract Awards	1,344.3	34.1%
- Local Share of Contract Awards	2,495.2	65.9%

Value of Finance Charges Incurred (based on prorated share)	\$132.3 million
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Summary of Federal Grants Received (FFGA)

Grant No.	FTA Award Date	FFY	Obligation Amount	Disbursement
NY-03-0344	22-Jun-99	1998	\$19.9	\$19.9
		1999	23.8	23.8
NY-03-0344-01	25-Apr-00	2000	2.0	2.0
NY-03-0344-02	14-May-01	2001	7.9	7.9
NY-03-0344-03	13-Jun-02	2002	14.6	14.6
NY-03-0344-04	05-Aug-03	2003	13.3	13.3
NY-03-0344-05	21-Sep-04	2004	73.8	73.8
NY-03-0344-06	10-May-05	2005	65.2	65.2
NY-03-0344-07	27-Jan-06	2005	34.0	34.0
NY-03-0344-08	21-Dec-06	2006	333.2	333.2
NY-03-0344-09	01 Aug 07	2007	300.0	300.0
NY-03-0344-10	11 Aug 08	2008	210.7	81.5
NY-36-0002 (ARRA)	01-Mar-10	2010	195.4	64.5
Subtotal ESA New Starts Grants			\$1,239.8	\$1,033.7
NY-95-X002 ESA (CMAQ)	14-Sept-07	2007	6.6	6.6
NY-90-X467 (MNR&LIRR)	29-Mar-02	2002	15.2	15.2
NY-03-0395 (LIRR)	20-Feb-02	2002	7.5	7.5
NY-03-0406 (MNR)	12-May-03	2003	5.0	5.0
NY-90-X489 (MNR)	08-Aug-03	2003	1.1	1.1
NY-03-0427 (LIRR)	01-Mar-05	2005	2.0	2.0
NY-05-0108 (LIRR)	27-Jun-06	2006	2.7	2.7
NY-05-0109 (LIRR)	24 Jul -07	2007	5.8	5.8
NY-95-X009 ESA (CMAQ)	13-Jan-09	2008	2.3	2.3
NY-95-X015 ESA (CMAQ)	22-Sept-09	2009	2.3	0
Subtotal Non-New Starts Grants			\$50.5	\$48.2
Total All Grants			\$1,344.4	\$1,081.9

Status of Project Contingency

Status of Project Contingency (\$M)				
	Sept. '09 Baseline	Drawdown	Current Value	%*
AFI				
Harold	\$11.5	(\$0.5)	\$11.0	
Manhattan	43.8	23.7	67.5	
Queens	22.5	(3.6)	18.9	
Systems	31.1	(1.2)	29.9	
Total AFI	\$108.9	\$18.4	\$127.3	5.4%
AWO (MODS)				
Harold	\$61.1	\$3.6	\$64.7	
Manhattan	86.3	(11.6)	74.7	
Queens	61.4	2.8	64.2	
Systems	32.6	(1.2)	31.4	
Total AWO (MODS)	\$241.4	(\$6.4)	\$235.0	5.6%
Construction Reserve	\$0.0	\$24.1	\$24.1	
Engineering	6.7	2.0	8.7	
OCIP	18.3	0.0	18.3	
Real Estate	33.8	0.0	33.8	
Rolling Stock	15.3	0.0	15.3	
Total	\$424.4	\$38.1	\$462.5	

* AFI % calculation represents the current value of AFI contingency as a percent of total unawarded construction. AWO (MODS) % calculation is the current value of contingency as a percent of total active/unawarded work to go for that category

Notes: Value include both third party and force account (direct + indirect) contingency

Contingency Drawdown

(\$ in Millions)

September 2009 Baseline	424.4
Fourth Quarter '09 Adjustments:	(33.5)
Fourth Quarter '09 Balance:	390.9
First Quarter '10 Adjustments:	
CH053 Contract Modifications	(1.6)
CQE28-1 Contract Modifications	(0.5)
Move conduit bank #62 (FM216)	(1.3)
CM008A Contract Modifications	(0.3)
CS780 Contract Modifications	(0.1)
Add'l work FHL01 (865 switch installation, interim comms work, trough installation)	(1.9)
VHA01 and VHL01 completion	0.5
CS800 Contract Modifications	(0.1)
2008/2009 Escalation Analysis Results	76.9
Total First Quarter '10 Adjustments:	71.6
Total	\$462.5

Force Account Construction Budget *(As of Mar 31, 2010)*

Description	Labor & Equip.	Materials	Total
Amtrak	\$139.7	\$45.3	\$185.0
LIRR	252.3	133.0	385.3
MNR	79.6	-	79.6
NYCT	7.2	-	7.2
NYAR	1.0	-	1.0
Force Account Estimate Total	\$479.7	\$178.3	\$658.0

Force Account 2010 Commitment Summary *(As of Mar 31, 2010)*

Description	'10 Plan	Actual	Remaining
Amtrak	\$11.1	\$1.0	10.1
LIRR	35.3	0	35.3
MNR	0	0	0
NYCT	1.3	1.3	0
NYAR	-	-	-
Force Account Estimate Total	\$47.7	\$2.3	\$45.4

Changes from Last Quarter

- \$1.3 million committed for FQT39
- \$1.0 million committed for VHA01

Second Quarter '10 – Look Ahead

Design

The Active Design Packages Table

Contract	Description	Design Start	Next Milestone & Date	Comments
Manhattan				
CM012	Manhattan Structures 2 & Facilities Fit-Out	N/A	90% 06/10	
CM013A	55th St Ventilation Facility	N/A	90% 08/10	Alternative 30% design received 04/10.
CM013B	38th St Ventilation Facility	N/A	100% 03/10	100% Submittal received 03/10.
CM014	GCT Concourse & Facilities Fit-Out	N/A	90% 08/10	On-board review of pre-90% design completed.
Queens				
CQ032	Plaza Substation & Queens Structures	N/A	100% 06/10	Substation B10 90% submittal received 03/10.
VQ065A	Loop Interlocking CIL	N/A	60% 04/10	
FQA65	Loop Interlocking – Amtrak F/A	N/A	60% 06/10	
Harold				
CH057	Harold Structures Part 3A	N/A	90% 07/10	Constructability review in progress.
CH058	Harold Structures Part 3B	N/A	60% 06/10	
FHA02	Harold Stage 2 Amtrak F/A Communications	N/A	100% 05/10	90% Submittal received 02/10.
FHA02A	Harold Stage 2A Catenary	N/A	30% 05/10	Resubmittal of 30% design.
FHL02	Harold Stage 2 LIRR F/A Communications	N/A	100% 05/10	90% Submittal received 02/10.
FHL02	Harold Tower Renovation	N/A	100% 02/10	100% Submittal received 02/10.
Systems				
CS078	Track, Third Rail & Tunnel Bench Walk	N/A	100% 06/10	90% Submittal received 02/10.
CS079	Communications, Controls, Security & Fire Detection	N/A	90% 03/10	Systems only.
CS084	Traction Power	N/A	100% 07/10	90% Submittal received 02/10.
CS086	Signal ESA Tunnels	N/A	60% 11/10	30% Submittal received 01/10.

Grand Central Terminal

- Work with the MTAPD to execute the conformance forms documenting their acceptance of architectural, mechanical and electrical space requirements at the ESA concourse level
- Provide a recommendation to MTACC resolving ADA accessibility issues impacting street level access to the ESA concourse level

Railroad Systems

- Continue development of Tunnel Misroute Protection Plan with LIRR
- Issue Comprehensive System Testing Plan (Volumes 1 and 2)
- Incorporate system packaging scheme into the Project Packaging Plan
- Finalize Tunnel Simulation Summary Report

Operational Readiness/LIRR Start Up

- Submit Operational Readiness Management Plan (Revision 1) to FTA/PMOC
- Complete Draft Revision 0 - Master Training Plan and submit to FTA/PMOC
- Develop draft of the Rail Activation Plan – Volume 1 and submit to FTA/PMOC
- Formulation of the strategy and components for the database that will be used for verifying Operational Readiness and Rail Activation process
- Continue identifying components and processes for developing the Asset Inventory, in coordination with the Long Island Rail Road
- Formulating outline of Volume 2 – Master Training Plan
- Complete of Revision 0 - Emergency Preparedness Plan and submit to FTA/PMOC
- Identifying interfaces between Comprehensive System Test Plan and Rail Activation Plan
- Documents under review by FTA/PMOC for comments:
 - Operations & Maintenance Plan (Revision 5)
 - Asset Management Plan (Revision 0)

Construction

- *Manhattan Tunnels Excavation (CM009)* – Complete mining with Robbins Robbins TBM at Lower drive 4. Begin excavation through concrete plug and continue mining with SELI at Lower drive 3 and begin blasting at Cross passage # 5.
- *Manhattan Structures 1 (CM019)* – Complete blasting at Shaft 3 & Shaft 4, continue blasting at Access tunnel 3 & 4 and mechanical excavation in Shaft 2 and Wellway 4 & Wellway 3. Continue excavation in caverns with both roadheader and drill/blast.
- *Madison Yard Site Clearance (CM008A)* – Completion of footing, concrete masonry unit (CMU) and fencing work at East & West perimeter wall. Completion of work for 46th

street sewer relocation, 45th street cross/steam tunnel modification and installation of new drainage lines.

- *44th St. Demolition & Construct Fan Plant Structure & 245 Park Ave. Entrance (CM004)*
 - *44th Street Vent Plant:* Demolish existing building, complete soil excavation, underpinning of adjacent buildings, install soldier piles and support of excavation. Commence channel and line drilling, and utility relocations.
 - *245 Park Ave Entrance:* Complete demolition, excavations, casting footings, steel erection & encasement, track-work (remove existing & install new track-slab & track). Commence installing new walls & support structure for new MNR platform & new 245 Park Ave Entrance stairs & escalator. Perform misc utility relocations & installations.
- *50th Street Ventilation Facility (CM013)* – Start excavating test pits for gas and steam utility lines on 50th St.
- *MNR Traction Power MODs & 13.2 kV Loop (FM216)* – Continue the installation of MODs and associated conduit/cable. Replacement of Transformer Houses Feeders, Installation of new positive and negative feeders, and Con Edison utility composite.
- *GCT Protection Work (CS790)* – Continue with concrete repair and netting system installation at GCT upper level
- *Queens Instrumentation (CS810)* – Continue drilling instrumentation in Harold Interlocking and Sunnyside Yard
- *Queens Bored Tunnels and Structures (CQ031)* – Continue open-cut excavation. Continue installation of mini-piles at 43rd St. Bridge. Begin construction of Tri-vent structure. Begin construction of Yard Lead Emergency Exit and Yard Lead Pit.
- *Northern Boulevard Crossing (CQ039)* – Complete slurry wall repair around Early Access Chamber and begin installation of retrofit bracings
- *Harold Structures Part 1 and G02 Substation (CH053)* – Continue G02 substation construction, installation of Harold Access Bridge and catenary structure foundations. Complete Tri-vent utility relocation. Begin construction of West Bound Bypass Bridge abutment.
- *Harold Structures Part 2A (CH054A)* – Continue construction of Retaining Wall THOM-S2 and begin construction of sewer line from the manhole 11 toward East
- *Harold Stage 1 (FHA/L01)* – Continue relocating catenary wire to new structures and circuit revision and testing for 865 switch cutover
- *Harold Early Stage 2(FHA/L02)* – Continue installing troughs, conduits and pull boxes for the stage 2 works and relocating communication cable at Harold South/North
- *F Interlocking CIHs (FHA62)* – Work on F2 and F1 are progressing to their respective October and January completion dates. Wiring changes required as part of Amtrak’s design updates will be made in the field by Amtrak force account.

- *Harold and Point CILs (VH051 Part 1)* – Continue work on POINT Interlocking submittals and staging and sequencing the CIL location installations.
- *Harold Tower Supervisory Control System (VH051 Part 2)* – Work with ARINC to mitigate impacts from the addition of the FHACS and prepare for factory acceptance testing beginning on May 17th at ARINC's facility in Marina del Rey, CA.

Budget and Finance

- Obtain the FTA's conditional approval on the programmatic documents
- Submit a draft Recovery Plan to the FTA
- Further development of Budget reports to track and report on Budget adjustments, contingency drawdown and estimates to complete

Executive Level Project Execution Plan

- Finalize the Cost and Cost Contingency Management Plan
- Finalize the Schedule and Schedule Contingency Management Plan
- Finalize Risk Mitigation White Paper

Quality

- Continue monitoring of contractor's internal and external audits
- Continue surveillances of each contract
- Scheduled audit of the GEC Quality Assurance and Quality Control process
- Additional training for CM staff
- Continue the review of all programmatic documents that incorporate new processes or enhance existing processes
- Refine as-built information process and implement tracking of information and revised design drawings

Methods and Procedures

- Continue to facilitate the development and completion of critical procedures

Safety

- Continue performing oversight of contractor's safety and security activities
- Implement ESA Safety Team's requirements and adjustment to shifting and expanding field conditions
- Continue to evaluate hazard analyses and safe work plans

Site and Systems Security

- Continue site visits with the FDNY to ensure that potential hazards are identified and addressed.

- Ensure system safety certification packages are submitted for committee review on *Traction Power (CS084)*, *Track, Third Rail and Tunnel Bench Walk (CS078)*, and *Communication, Controls, Security and Fire Detection (CS079)*.
- Convene Technical Life Safety Working Group to present the Schirmer means of egress report for both the caverns and concourse.
- Facilitate coordination with the FDNY and MNR railroad to identify space for an Incident Command Center (ICC) in the vicinity of 43 and Vanderbilt.

Real Estate

- Conduct research in support of an agreement for rebuilding of dilapidated retaining wall at 30-25 Queens Boulevard – Gaseteria
- Research property ownerships north of Mid Day Storage Yard and facilitate the removal of encroachments *Mid-Day Storage Yard Facility (CQ033)*
- Meet with designers to determine real estate needs for *55th St. Ventilation Facility (CM013A)*
- Progress acquisition of additional Queens easements for *Harold Structures Part 3 (CH058)*

Public Outreach

- Work with NYCDOT and NYPD to assign traffic agents to 37th Street and Park Avenue and 50th Street during surface construction
- Conduct outreach for *Queens Tunnels Excavation (CQ031)* and *Northern Boulevard Crossing (CQ039)*
- Conduct additional pre-construction surveys within vicinity of *50th Street Ventilation Facility (CM013)*