

Park Avenue Viaduct Replacement Project Presentation to Community Board 11



June 7, 2021

Agenda

- Introductions
- Project Purpose and Need
- Conceptual Construction Renderings
- Anticipated Project Schedule
- Stakeholder and Community Coordination



What is the Park Avenue Viaduct?

The Park Avenue Viaduct is an elevated steel structure built in 1893 (128 years ago) which carries four Metro-North Railroad tracks above Park Avenue in East Harlem.

- 98% of all Metro-North trains use the viaduct
- 750 trains and 220,000 customers use the viaduct on a typical (Pre-Covid) weekday
- 1.25-mile-long steel structure (5 track miles)

The viaduct is the main artery that carries all riders to and from Grand Central Terminal and Harlem-125th Street Station.



Why is the Viaduct important to the Harlem Neighborhood?

- Viaduct served 5.3 million customers at Harlem-125 Street Station in 2019
- Harlem-125th Street Station is 3rd busiest in Metro-North system (Pre-Covid)
- \$24 Million in recent station investments



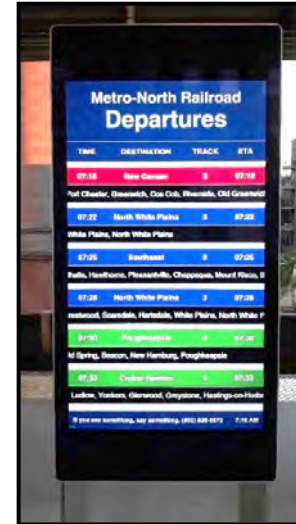
Harlem 125 St. Neighborhood Investments



125th St. Lighting Improvement Project (SBMP)



125th St. Enhanced Station Initiative (ESI)



125th St. Customer Service Initiative (CSI)



125th St. Stair Replacement Project (SBMP)



E. 132nd St. Viaduct Painting (SBMP)

More than \$24 Million recently Invested



What is the Park Avenue Viaduct Replacement Project?

MTA Construction & Development and Metro-North Railroad plan to **replace** major segments of the Park Avenue Viaduct between E115th Street and E132nd Street along Park Avenue.

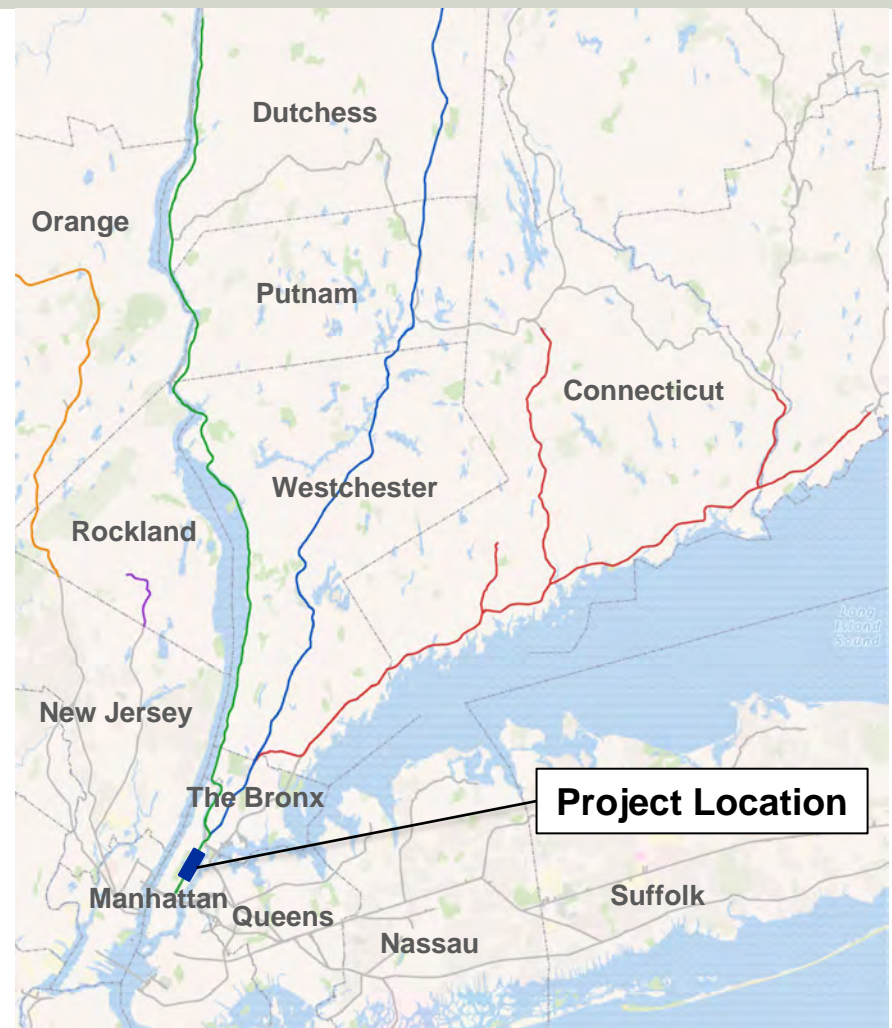
- State of Good Repair project
- No change to the existing number of tracks



Project Need

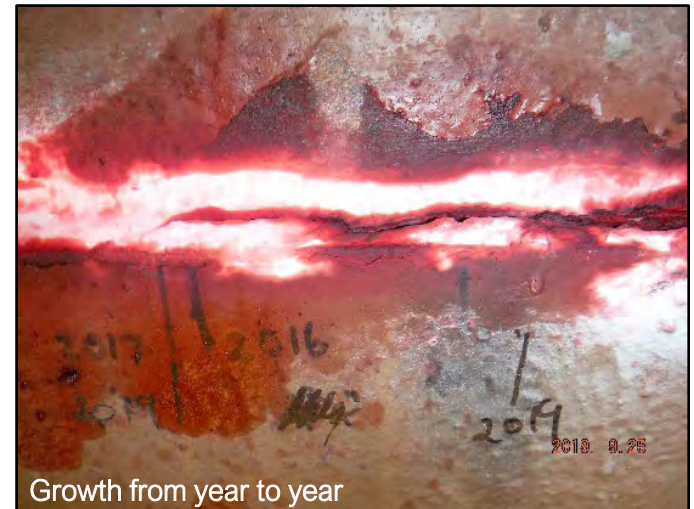
All MNR East of Hudson trains must pass over the Viaduct to reach **Harlem-125th Street Station** and **Grand Central Terminal** – *there are no alternate routes.*

- The PAV is not in a state of good repair
- Current usage is well above original design
- PAV is a single point of failure for East-of-Hudson service
- Without PAV, MNR cannot serve The Bronx, Manhattan, Westchester, Putnam, Dutchess, or Connecticut



Importance of Acting Now

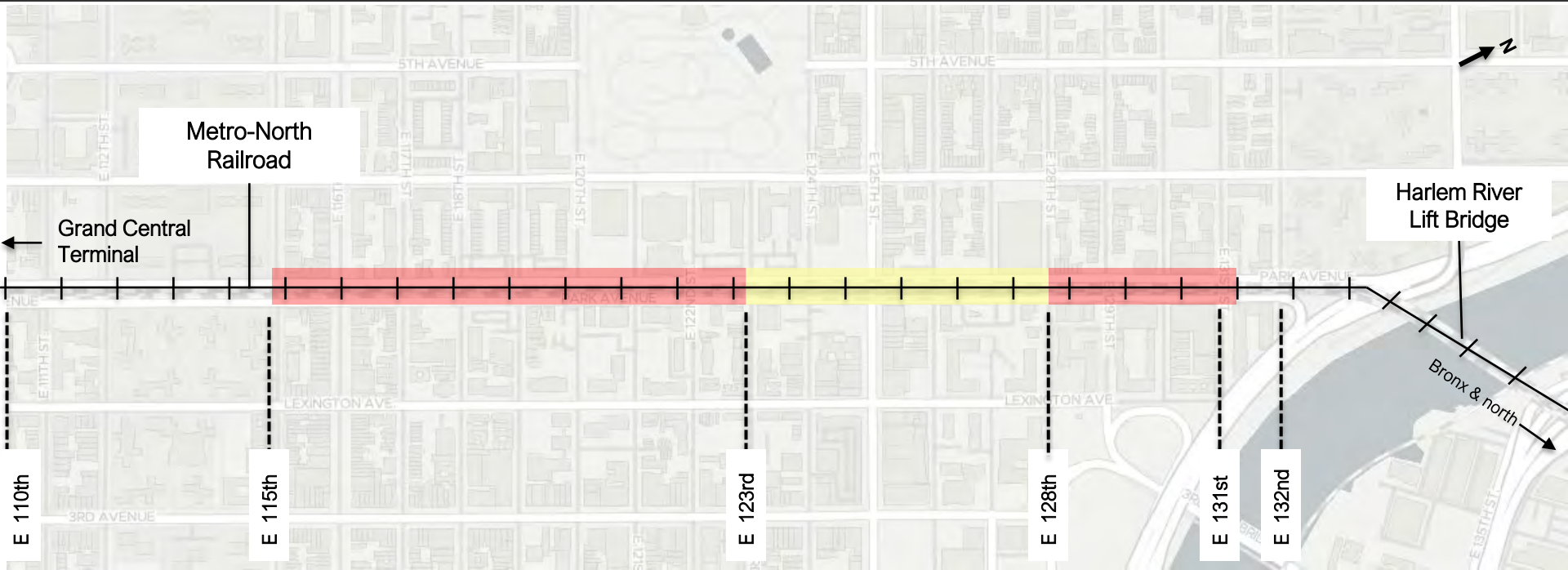
- PAV was built in 1893 (over 128 years old)
- Structure continues to exhibit signs of distress
- PAV is beyond its fatigue life
- Increasing annual maintenance costs
- Public trust in MNR infrastructure must be ensured



Growth from year to year



Park Avenue Viaduct Project Area



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High Priority: Interim Repairs and Full Replacement of Viaduct ■

Rehabilitation ■





PAV Safety and Repair Measures



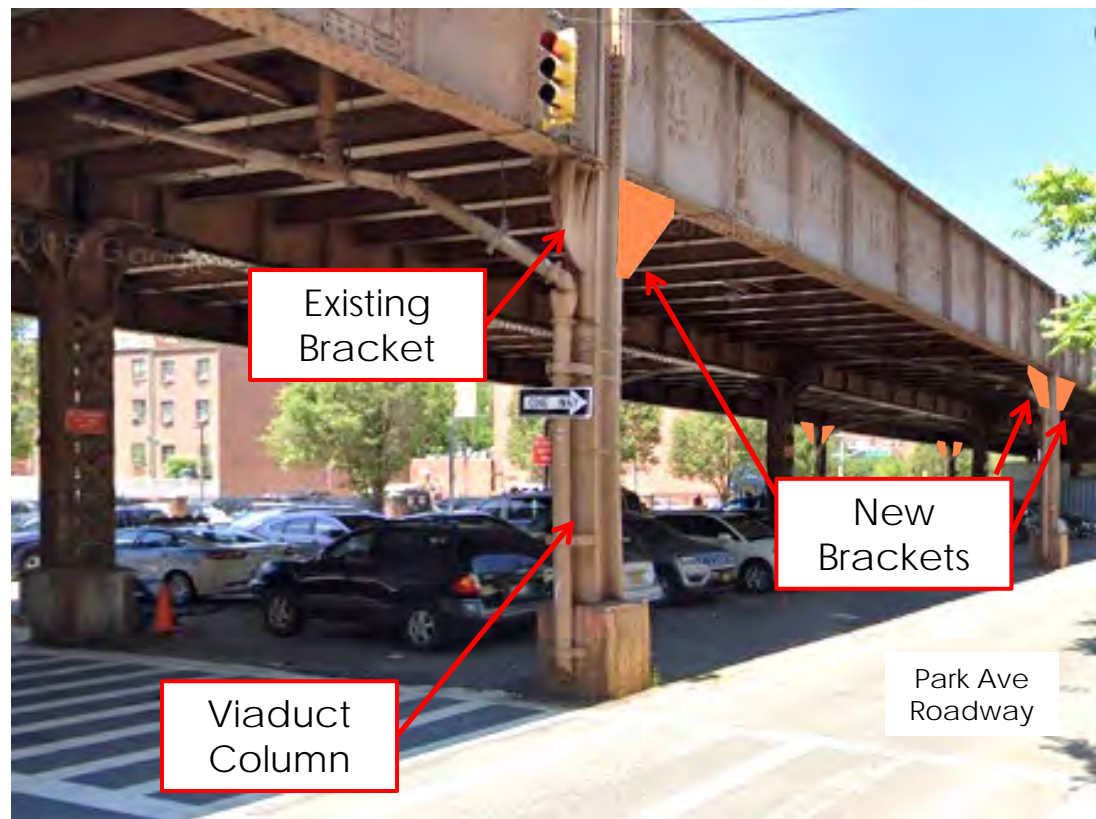
Ongoing Short Term Safety Measures

- Inspections
 - Increased inspection frequency
- Testing
 - Continuation of non-invasive testing NDT (ultra-sonic & magnetic particle)
- Routine Maintenance & Flag Repairs
 - MNR Maintenance-of-Way (on-going)
- Interim Repair Project (2021)
 - Third Party Contractor



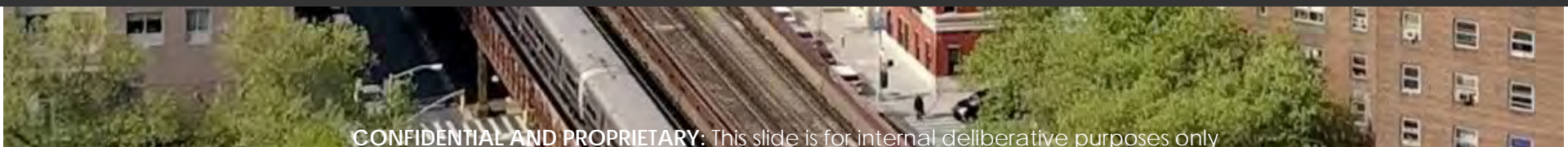
PAV Interim Repairs

- E115th to E123rd and E128th to E131st Street along Park Avenue
- Columns adjacent to Park Ave Roadway
- Repair Start: July 2021
- Repair End: April 2022
- New brackets to be installed at existing column locations
- Color to match existing
- Maintain existing 1 lane of traffic





Conceptual Construction Renderings



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Preconstruction (Lots Cleared)



Proposed method shown is a concept only. Actual details may be different.



Construct Foundations



Proposed method shown is a concept only. Actual details may be different.



Remove Existing Bridge with SPMT



Proposed removal method shown is a concept only. Actual means of bridge removal may be different.

Remove Existing Bridge with SPMT



Proposed removal method shown is a concept only. Actual means of bridge removal may be different.

Install Pier Caps



Proposed structure and installation shown is a concept only. Actual details may be different.

Install Bridge with SPMT (East 2 Tracks)



Proposed structure and installation shown is a concept only. Actual details may be different.

Install Bridge with SPMT (East 2 Tracks)



Proposed structure and installation shown is a concept only. Actual details may be different.



Completed Bridge Replacement



Proposed structure shown is a concept only. Actual details may be different.

Completed Bridge Replacement (All Tracks)



Proposed structure shown is a concept only. Actual details may be different.



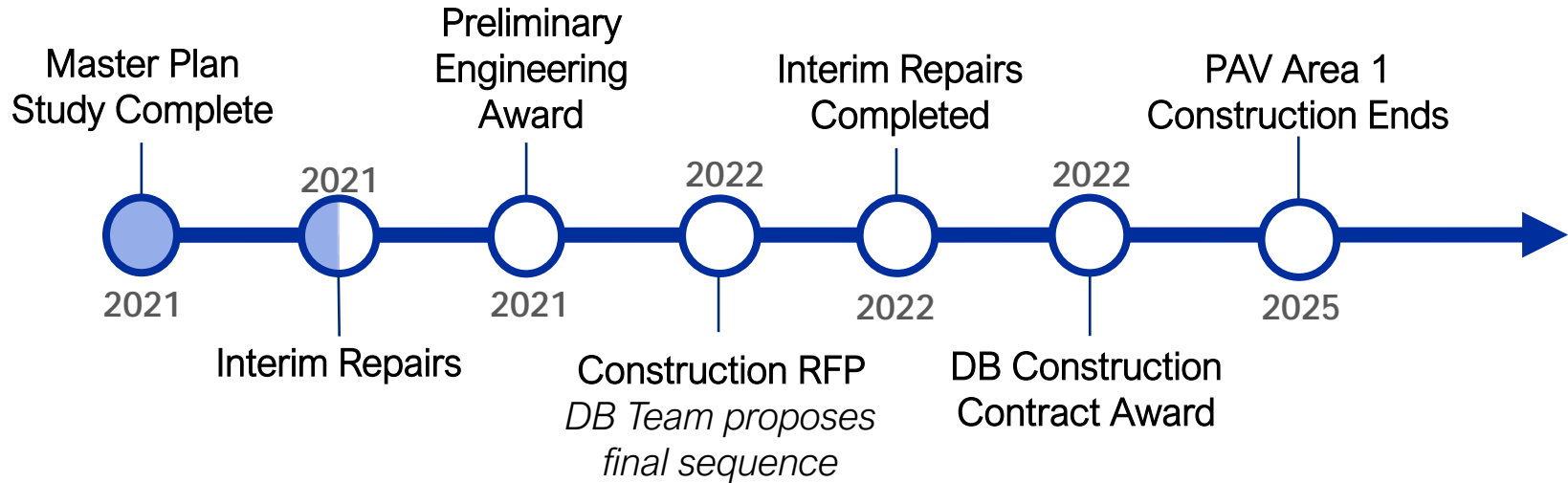


Anticipated Project Schedule



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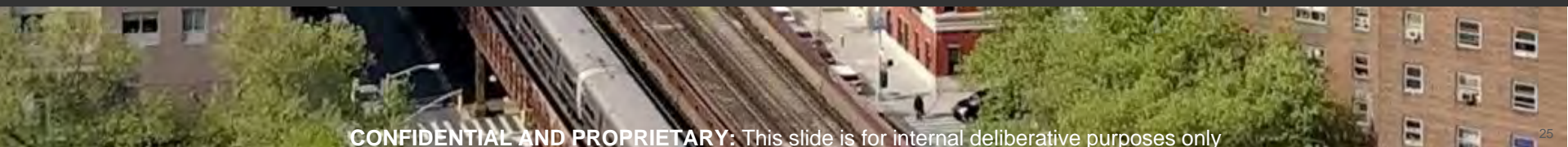
Anticipated Project Schedule*



*Schedule dependent on selected design-build contractor.



PAV Replacement Project Benefits



Project Benefits



Maintains the **safety** of the public and the railroad



Constructs a more resilient viaduct that meets modern industry design standards



Ensures train service to and from Harlem-125th Street Station & the Bronx; Westchester, Putnam and Dutchess Counties; and Connecticut



Includes tens of millions of dollars for DBE contracts



Installs new traffic signals and infrastructure



Introduces construction-induced spending and job creation



Provides unique opportunity to partner with local schools (STEM)



Mass transit reduces traffic congestion and improves air quality





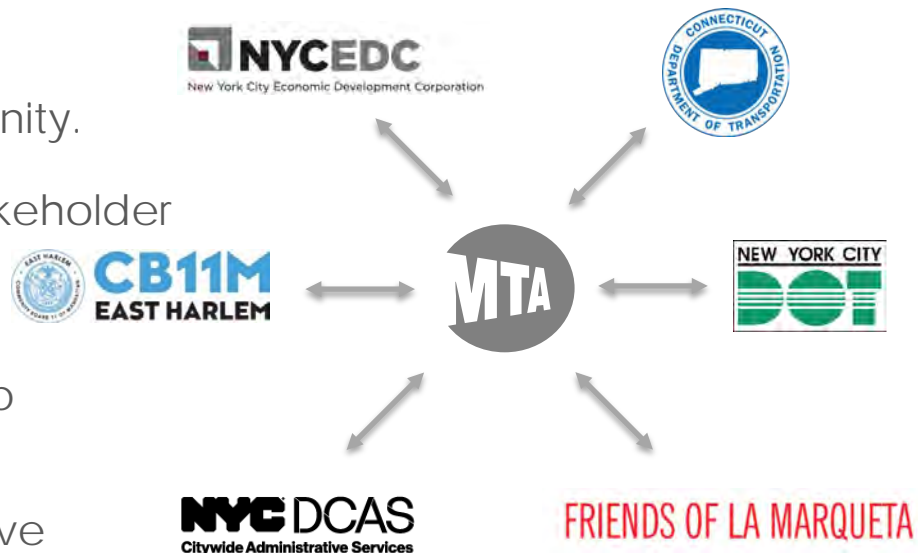
Stakeholder and Community Coordination



Community Coordination to Date

- MTA is committed to being a good neighbor and working with the community.
- MTA has been actively engaged in stakeholder outreach, coordinating with:

- Elected Officials
- NYC Economic Development Corp
- NYC Dept of Transportation
- NYC Dept of Citywide Administrative Services
- Community Board 11
- Friends of La Marqueta
- Other stakeholder groups



Community Coordination During Replacement

- MTA is planning a robust community outreach effort for the replacement project that will include, but is not limited to:
 - Project-dedicated email, webpage, and hotline
 - Regular communication with stakeholders (e.g., bilingual flyers, signage)
 - Regular meetings with:
 - Elected Officials
 - Partnering Agencies
 - Community and Business Groups
 - Transportation Groups and Commuters
 - Exploring potential for unique community partnerships





End of Presentation



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