



Metropolitan Transportation Authority

Construction & Development

Metro-North Railroad

MetLife

Park Avenue Viaduct Replacement Project Presentation to Community Board 11

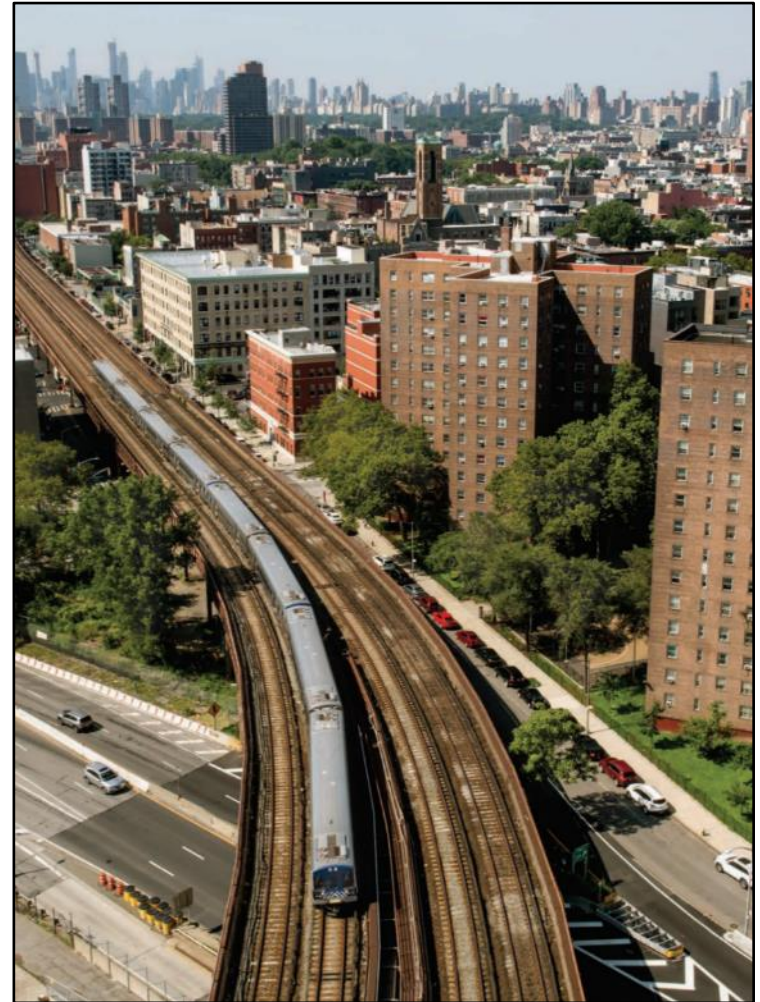


February 10, 2022

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Agenda

- Introductions
- Project Purpose and Need
- Anticipated Project Schedule
- Stakeholder and Community Coordination
- Discussion / Questions



What is the Park Avenue Viaduct?

The Park Avenue Viaduct is an elevated steel structure built in 1893 (129 years ago) which carries four Metro-North Railroad tracks above Park Avenue in East Harlem.

- 98% of all Metro-North trains use the viaduct
- 750 trains and 220,000 customers use the viaduct on a typical (Pre-Covid) weekday
- 1.25-mile-long steel structure (5 track miles)

The PAV is the main artery that carries all riders to and from Grand Central Terminal and Harlem-125th Street Station (See photo).



Why is the Viaduct important to the Harlem Neighborhood?

- PAV served 5.3 million customers at Harlem-125 Street Station in 2019
- Harlem-125th Street Station is 3rd busiest in Metro-North system (Pre-Covid)
- \$24 Million in recent station investments



Harlem 125th Street Neighborhood Investments



125th St. Lighting Improvement Project (SBMP)



125th St. Enhanced Station Initiative (ESI)



125th St. Customer Service Initiative (CSI)



125th St. Stair Replacement Project (SBMP)



E. 132nd St. Viaduct Painting (SBMP)

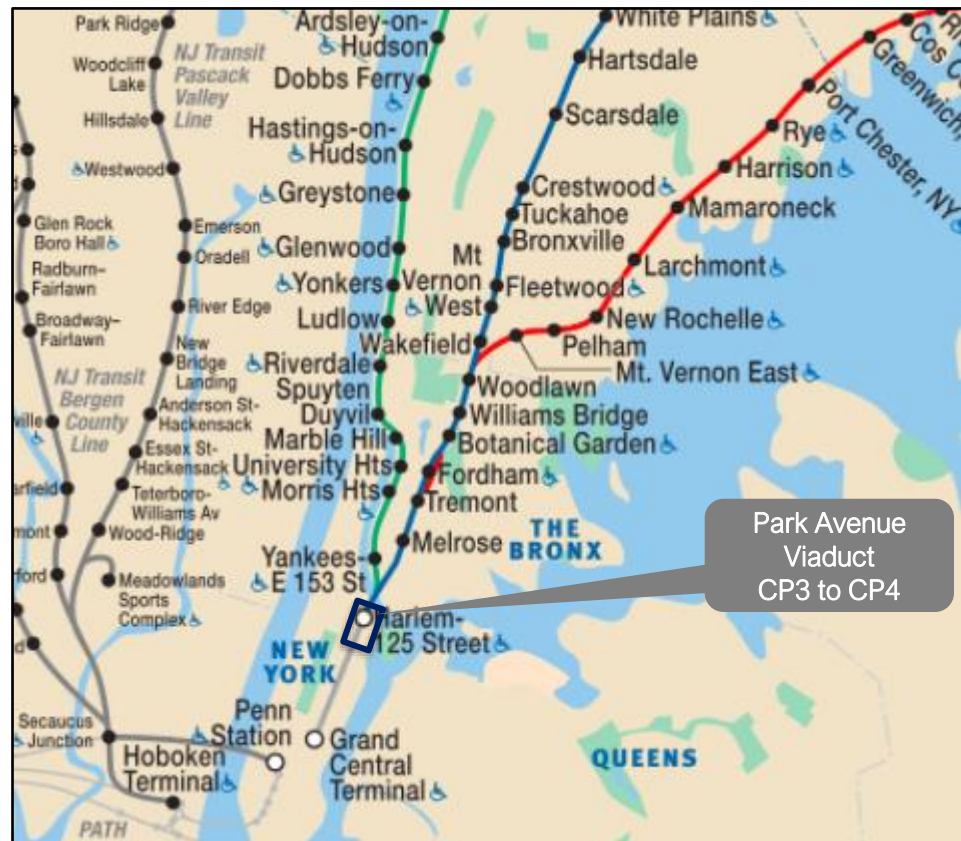
More than \$24 Million recently Invested



What is the Park Avenue Viaduct Replacement Project?

MTA C&D and Metro-North Railroad plan to **replace** major segments of the Park Avenue Viaduct between E115th Street and E132nd Street along Park Avenue.

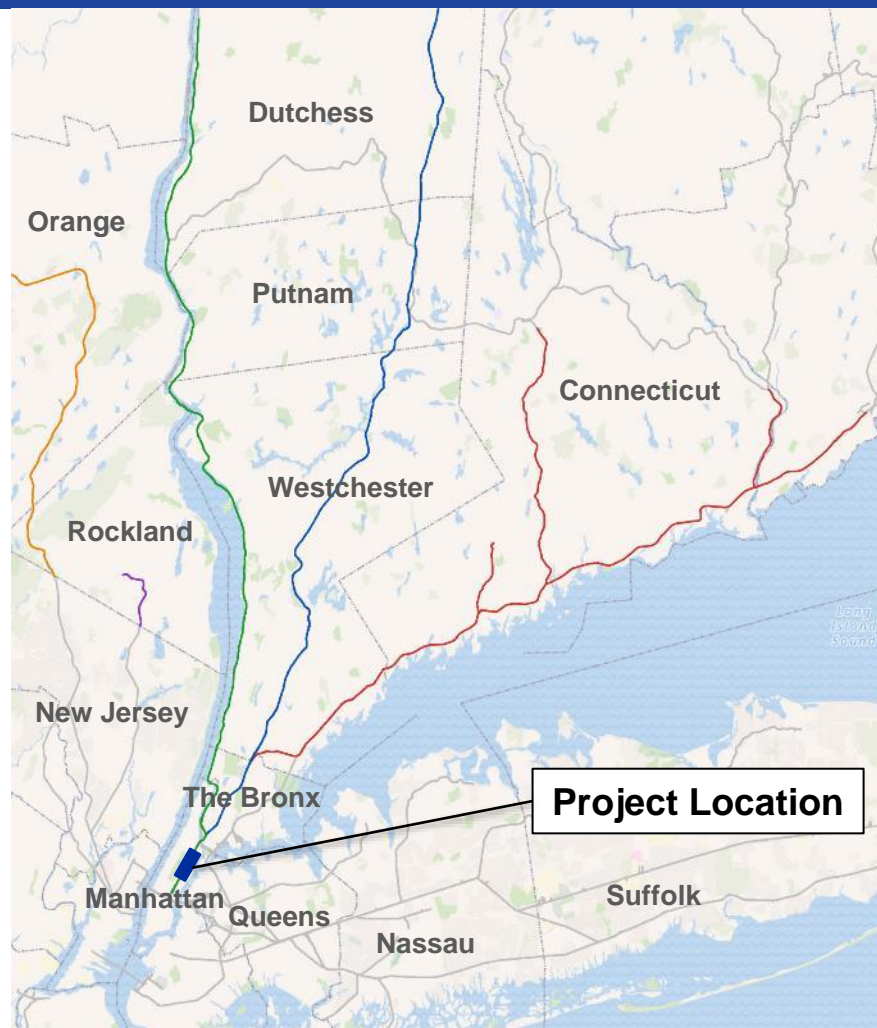
- State of Good Repair project
- No change to the existing number of tracks



Project Need

All MNR East of Hudson trains must pass over the Viaduct to reach **Harlem-125th Street Station** and **Grand Central Terminal** – *there are no alternate routes.*

- The PAV is not in a state of good repair
- Current usage is well above original design
- PAV is a single point of failure for East-of-Hudson service
- Without PAV, MNR cannot serve The Bronx, Manhattan, Westchester, Putnam, Dutchess, or Connecticut





Viaduct Repair and Replacement Plan

Importance of Acting Now

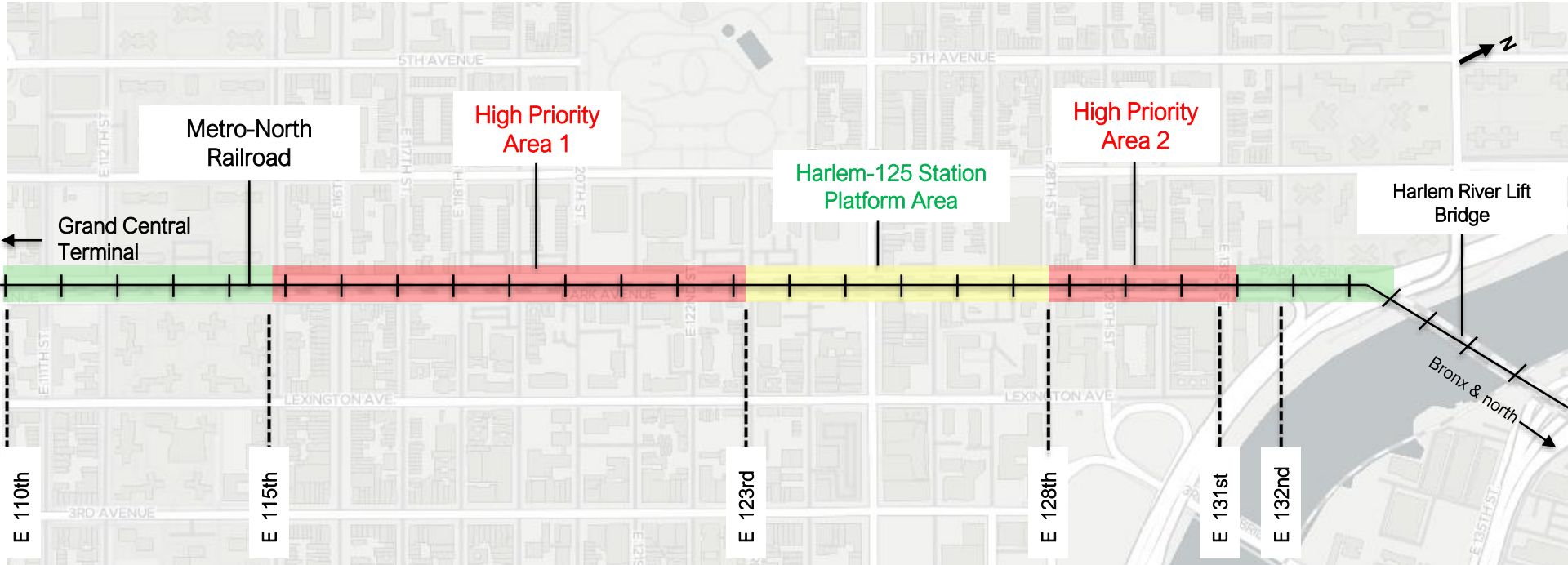
- PAV was built in 1893 (over 129 years old)
- Structure continues to exhibit signs of distress
- PAV is beyond its fatigue life
- Current usage is well above original design
- The PAV is not in a state of good repair
- Public trust in MNR infrastructure must be ensured



Growth from year to year



Park Avenue Viaduct Program Prioritization



© OpenStreetMap contributors

- High Priority: Interim Repairs and Full Replacement of Viaduct ■
- Rehabilitation ■
- Painting & Localized Repairs ■



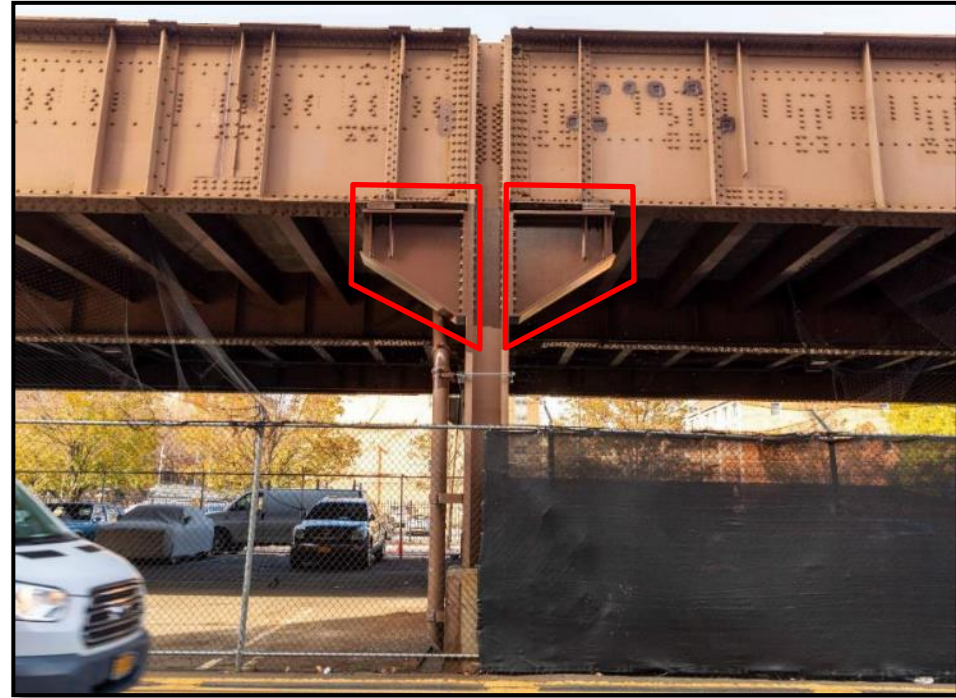
Ongoing Short Term Safety Measures

1. Inspections
 - In-depth inspections at 6-month intervals
 - Upcoming Spring 2022 cycle
2. Testing
 - Continuation of non-invasive testing (ultra-sonic & magnetic particle)
3. Routine Maintenance & Flag Repairs
 - MNR Maintenance-of-Way (ongoing)
4. Interim Repair (Retrofit) Project 2021-2022
 - Ahead of Schedule



PAV Interim (Retrofit) Repairs

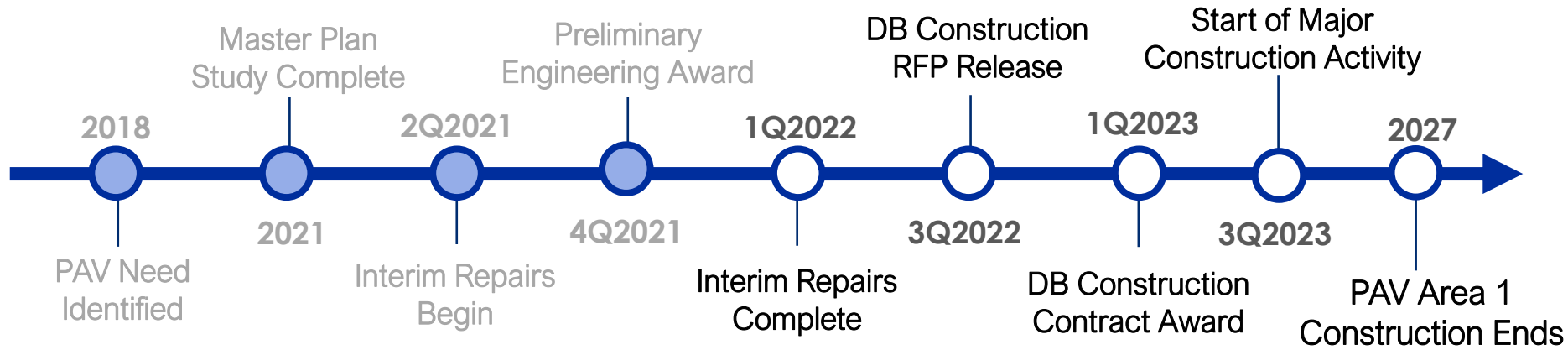
- E115th to E123rd and E128th to E131st Street along Park Avenue
- Columns adjacent to Park Ave roadway
- Maintained one lane of traffic
- Repair Start: July 2021
- Repair End: March 2022 (Painting)
- All 122 brackets installed Jan 2022 (4 Months Early)



An aerial photograph of a city street featuring elevated train tracks on the left. The tracks run parallel to a street with several cars. To the right of the tracks is a dense urban area with various multi-story buildings in different colors, including red brick, tan, and white. A prominent yellow building with many windows is visible in the center. The scene is captured from a high angle, showing the layout of the street and the surrounding city blocks.

Anticipated Project Schedule & Benefits

Anticipated Project Schedule*



**Schedule dependent on selected design-build contractor.*



Project Benefits



Maintains the **safety** of the public and the railroad



Includes tens of millions of dollars for DBE contracts



Constructs a more resilient viaduct that meets modern industry design standards



Integrate DOT improvements



Ensures train service to and from Harlem-125th Street Station, GCT, The Bronx, & stations north



Introduces construction-induced spending and job creation



Potential for reduced noise and vibration levels



Provides unique opportunity to partner with local schools (STEM)



MTA Arts & Design component



Mass transit reduces traffic congestion and improves air quality



Sustainable materials





Public Outreach

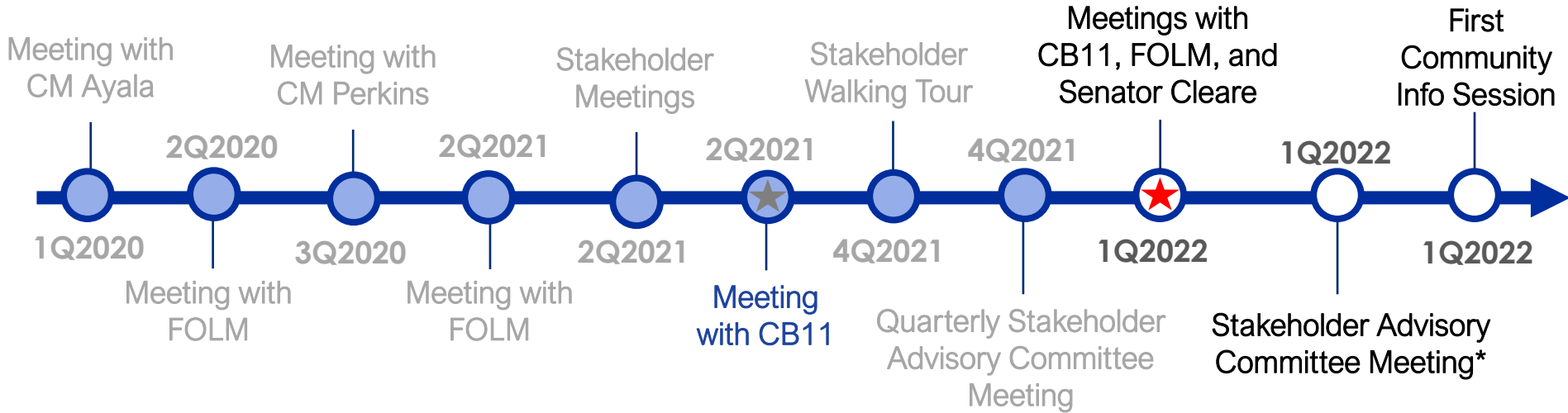


Community Coordination

- MTA is committed to being a good neighbor and working with the community.
- MTA has been actively engaged in stakeholder outreach, coordinating with:
 - Elected Officials
 - City and State Agencies
 - Stakeholder Groups



Stakeholder Outreach Schedule



**PAV Stakeholder Advisory Committee Meetings will be held quarterly.*



Summary of Stakeholder Input Received to Date

| Design Considerations | Status |
|--|---|
| Lighter color viaduct | Ongoing discussions with SHPO |
| Improve lighting underneath the viaduct | Ongoing discussions with DOT |
| Increase pedestrian safety | Ongoing discussions with DOT |
| Install bollards between columns | Ongoing discussions with DOT |
| Open columns (not solid pier walls) | Revisit during Preliminary Engineering |
| Compatibility with La Placita | Revisit during Preliminary Engineering |
| Improvements to La Placita | Must adhere to federal funding requirements, ongoing discussions with NYC EDC |
| Attenuate operational noise/vibration | Revisit during Preliminary Engineering |
| Incorporate stormwater drainage & green infrastructure | Revisit during Preliminary Engineering |



Summary of Stakeholder Input Received to Date (Page 2)

| Construction Related Considerations | Status |
|---|--|
| Coordination with adjacent HPD projects | Ongoing consultation with HPD |
| Level of noise during construction | Noise will be monitored during construction and community will remain apprised of activities |
| Air quality during construction | Air quality will be monitored during construction |
| Use visually pleasing temporary construction barriers | Revisit during Preliminary Engineering |
| Business Related Considerations | |
| Encourage local hiring and purchasing | Revisit during Preliminary Engineering. Must adhere to federal requirements |
| Minimize temporary disruptions to La Placita & businesses | Ongoing discussions with City Agencies |
| Maximize visibility of local businesses during construction | Revisit during Preliminary Engineering |



Summary of Stakeholder Input Received to Date (Page 3)

| Miscellaneous Considerations | Status |
|--|---|
| Improve 125 th Street Station Accessibility | Station is fully ADA accessible |
| 125 th Street Station TOD opportunities | Separate initiative from Viaduct Project |
| Public Outreach Considerations | |
| Continued public involvement | First community information session: 1Q2022 |
| Timely construction notifications | Committed to robust outreach program |



Arts & Design Program

- MTA Arts & Design commissions dynamic contemporary art for subway stations, commuter rail stations, and other projects



Harlem – 125th Street, Art Installation: Harlem Encore, 1999

Visit: <https://new.mta.info/agency/arts-design>



STEM Coordination

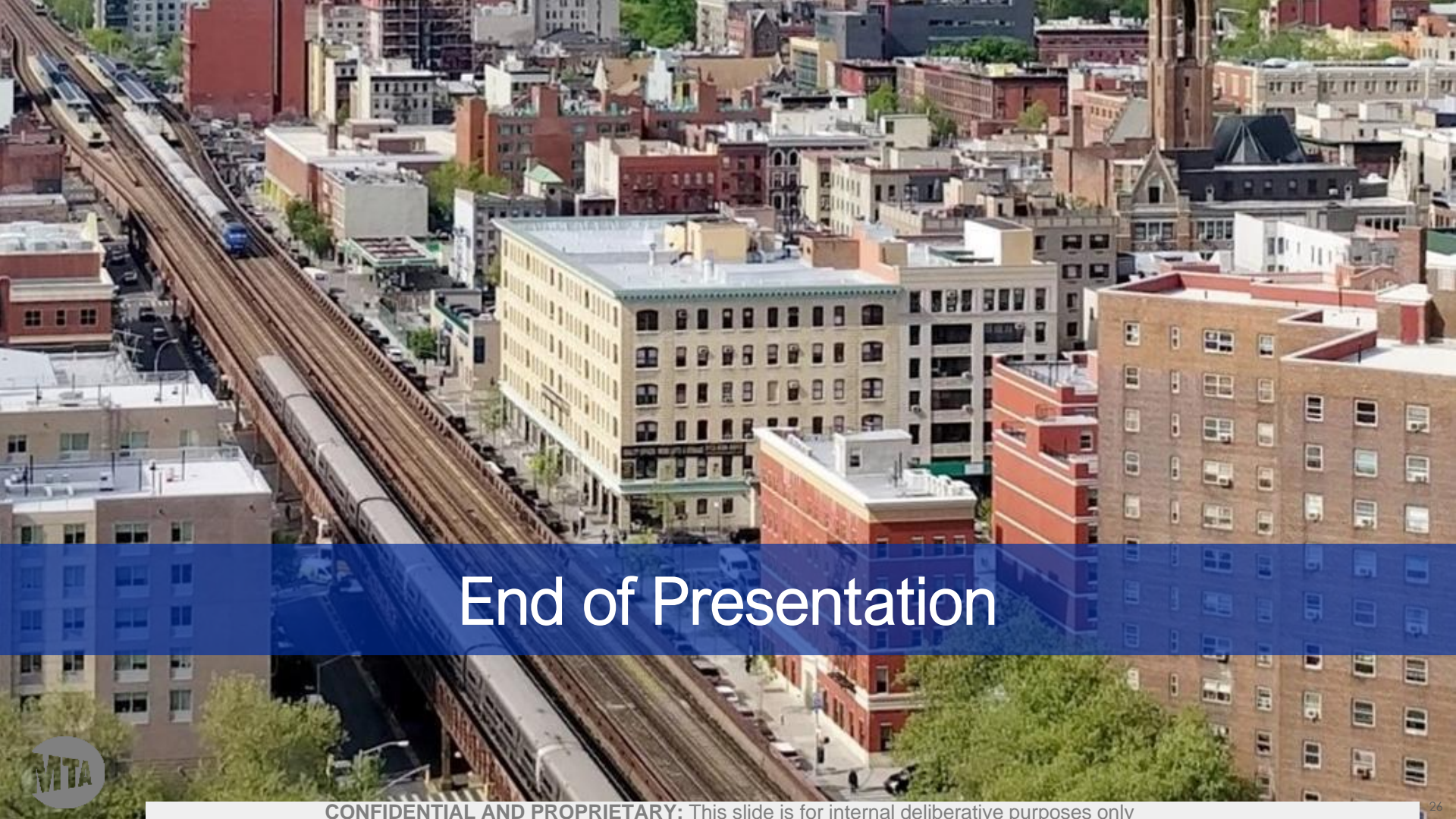
- STEM Program with local schools
- Mosaic Preparatory Academy (DOE K-5)
 - STEM Day on December 14, 2021
 - 5 classes (2nd through 5th grades)
- Urban Assembly School for Global Commerce (DOE 9-12)
 - Planning for in-school programming this year
 - Program to be expanded as the PAV project proceeds



A red MTA train is traveling on a viaduct in a city, likely New York City. The train is moving away from the viewer. The background is filled with tall, multi-story buildings. A sign on the left side of the viaduct reads "N.Y. 4".

MTA'S PARK AVENUE VIADUCT PROJECT **DECEMBER 14, 2021**

MOSAIC PREPARATORY ACADEMY STEM DAY!



End of Presentation

